



**North Liberty Planning Commission
Tuesday, April 5, 2022, 6:30 PM
North Liberty City Council Chambers
1 Quail Creek Circle, North Liberty, Iowa 52317**

This meeting may be accessed live by the public in person or on the internet at northlibertyiowa.org/live, on Facebook at facebook.com/northliberty or on YouTube at youtube.com/northliberty. Meetings are rebroadcast on cable and available on-demand on northlibertyiowa.org.

Call to Order

1. Roll Call

2. Approval of the Agenda

3. Comprehensive Plan Future Land Use Amendment: Request of the City of North Liberty to amend the Future Land Use Map on 25.64 acres, more or less, on property located on the east side of North Dubuque Street approximately 185' south of Scales Bend Road from Commercial and Industrial to Residential.

- a. Staff Presentation
- b. Public Comments
- c. Questions and Comments
- d. Recommendation to the City Council

4. Public Hearing on Zoning Map Amendment: Request of Daniel & Rhonda Bernacki Revocable Trust for a zoning map amendment (rezoning) on 25.64 acres, more or less, on property located on the east side of North Dubuque Street approximately 185' south of Scales Bend Road from C-2-A Highway Commercial District and I-1 Light Industrial District to RS-6 Single-Unit Residence District (19.73 acres) and RM-12 PAD Multi-Unit Residence District Planned Area Development (5.91 acres).

- a. Staff Presentation
- b. Applicant Presentation
- c. Public Comments
- d. Questions and Comments
- e. Recommendation to the City Council

- 5. Archie's Brake and Lube Site Plan:** Request of Archie's Rent A Ride, LLC to approve a Site Plan for a minor vehicle repair facility and related site improvements on approximately 1.5 acres of property located at 1040 Liberty Way.

 - a. Staff Presentation
 - b. Applicant Presentation
 - c. Public Comments
 - d. Questions and Comments
 - e. Recommendation to the City Council
- 6. Forevergreen Estates Preliminary Subdivision Plat:** Request of Bowman Property, LLC to approve a Preliminary Subdivision Plat for a 129 lot subdivision on 109.46 acres, more or less, on property located at the northeast corner of West Forevergreen Road and South Kansas Avenue.

 - a. Staff Presentation
 - b. Applicant Presentation
 - c. Public Comments
 - d. Questions and Comments
 - e. Recommendation to the City Council
- 7. Solomon Landings Preliminary Subdivision Plat:** Request of Solomon Holdings, LLC to approve a Preliminary Subdivision Plat for a 118 lot subdivision on 76.97 acres, more or less, on property located on the north side of West Penn Street west of the North Jones Boulevard as extended northerly from West Penn Street to 240th Street.

 - a. Staff Presentation
 - b. Applicant Presentation
 - c. Public Comments
 - d. Questions and Comments
 - e. Recommendation to the City Council
- 8. The Emory at North Liberty Site Plan:** Request of Insite Construction to approve a Site Plan for 352 multi-unit residences in eight buildings and related recreational amenities and site improvements on approximately 25.11 acres of property located on the west side of North Jones Boulevard as extended northerly from West Penn Street to 240th Street.

 - a. Staff Presentation
 - b. Applicant Presentation
 - c. Public Comments
 - d. Questions and Comments
 - e. Recommendation to the City Council

9. Public Hearing for Zoning Code Amendment: Request of the City of North Liberty for an Ordinance for an Ordinance amending Section 167.01 regarding the definition of "Masonry, Required" and Section 168.07 regarding use standards for garages attached to dwellings and scrivener errors in use standards for vehicle dealerships.

- a. Staff Presentation
- b. Public Comments
- c. Questions and Comments
- d. Recommendation to the City Council

10. Approval of Previous Minutes

11. Old Business

12. New Business

13. Adjournment



To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of the City of North Liberty to amend the Comprehensive Plan Future Land Use Map on 25.64 acres, more or less, on property located on the east side of North Dubuque Street approximately 185' south of Scales Bend Road from Commercial and Industrial to Residential.**

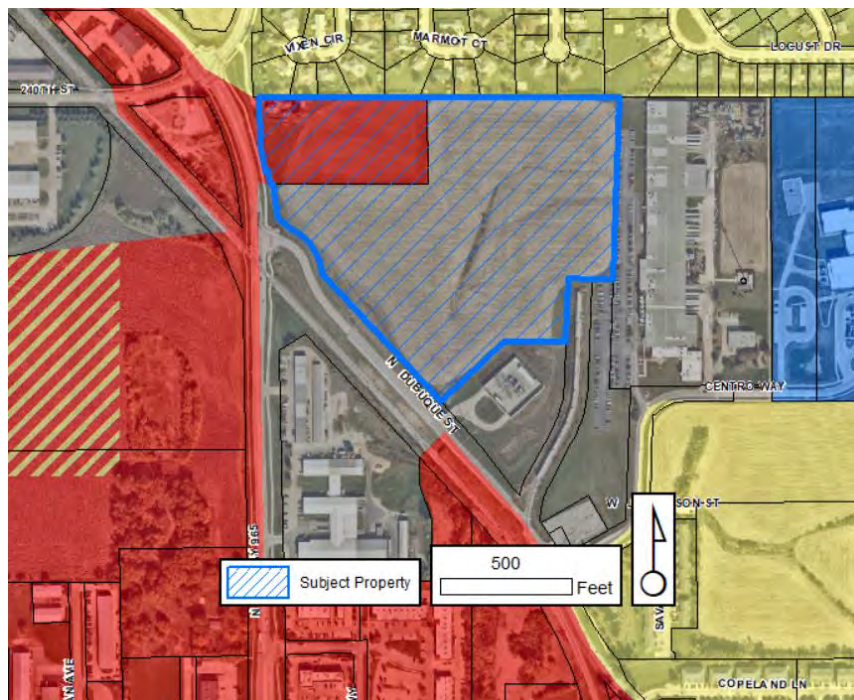
North Liberty City staff has reviewed the subject submission, and offer comments presented in this memo. The staff review team includes the following personnel:

Ryan Heiar, City Administrator
Tracey Mulcahey, Assistant City Administrator
Grant Lientz, City Attorney
Tom Palmer, City Building Official
Kevin Trom, City Engineer
Ryan Rusnak, Planning Director

1. Request Summary:

Staff is requesting a Comprehensive Plan Future Land Use Map amendment from Commercial and Industrial to Residential so the forthcoming RS-6 Single-Unit Residence District and RM-12 PAD Multi-Unit Residence District Planned Area Development rezoning would achieve consistency with the Comprehensive Plan.

2. Current Land Use Map designation: Commercial and Industrial.



The North Liberty Comprehensive Plan articulates the following regarding residential uses:

The plan promotes the development of a diversified housing stock that is affordable to a wide range of incomes. Even though general planning goals include limiting residential uses along arterials and in some cases even collector streets, the miles of such frontages within the City make strict adherence to those goals impractical, and landscape buffers, limited access, and smart neighborhood street layouts are utilized to minimize traffic impacts. Higher density residential development is considered to be somewhat of a transitional buffer between office/commercial development and lower density residential neighborhoods, in part because it is practical to locate the greater numbers of residents found in the higher density developments closer to the commercial services they need.

Relevant Comprehensive Plan Policies Related to Land Use:

- Protect residential neighborhoods from encroachment or intrusion of incompatible higher use types by adequate buffering and separation. This policy is not to be interpreted to imply that new development must match existing development in cost, density, or character; but instead to mean that gross incompatibilities shall be minimized and mitigated where unavoidable.

3. Public Input:

No public input has been received specific to this request, however, public input has been provided with the proposed rezoning.

4. Discussion:

The initial phase of Centro, Inc. was constructed in 1990 and it appears that the surrounding area was envisioned to the industrial area at the north end of North Liberty as there weren't adjacent home sites. The initial phase of Fox Run was platted in 1995.



1990 Zoning Map

The City of North Liberty has grown around this area calling into question future industrial development in this immediate area. The contemporary trend for industrial development is close proximity to the interstate. It is staff's opinion that a Residential Land Use would be much more compatible with the surrounding area. As the new Comprehensive Plan gets developed, the Industrial Future Land Use and industrial zoning west of Ranshaw Way will be more closely examined as well.

5. Staff Recommendation:

Findings:

1. The Residential Land Use and residential development in this location would be more compatible with the surrounding area; and
2. The Residential Land Use in this location would achieve consistency with adopted Comprehensive Plan Land Use Policies.

Recommendation:

Staff recommends the Planning Commission accept the two listed findings and forward the Comprehensive Plan Land Use Amendment from Commercial and Industrial to Residential to the City Council with a recommendation for approval.

Suggested Motion:

I move that the Planning Commission accept the two listed findings and forward the Comprehensive Plan Amendment to the City Council with a recommendation for approval.



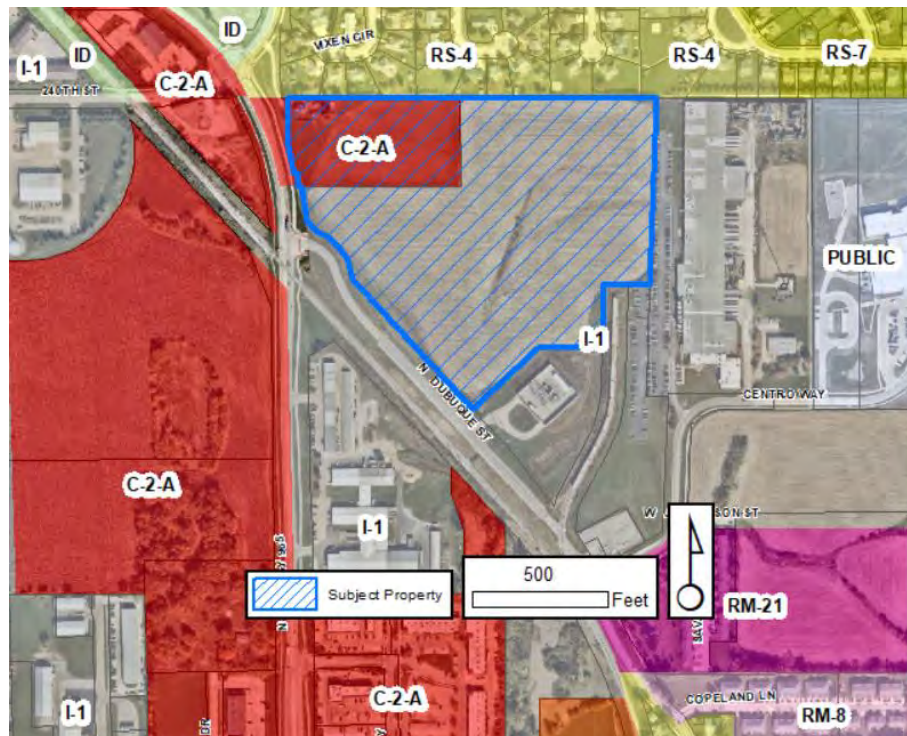
To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of Daniel & Rhonda Bernacki Revocable Trust for a zoning map amendment (rezoning) on 25.64 acres, more or less, on property located on the east side of North Dubuque Street approximately 185' south of Scales Bend Road from C-2-A Highway Commercial District and I-1 Light Industrial District to RS-6 Single-Unit Residence District (19.73 acres) and RM-12 PAD Multi-Unit Residence District Planned Area Development (5.91 acres).**

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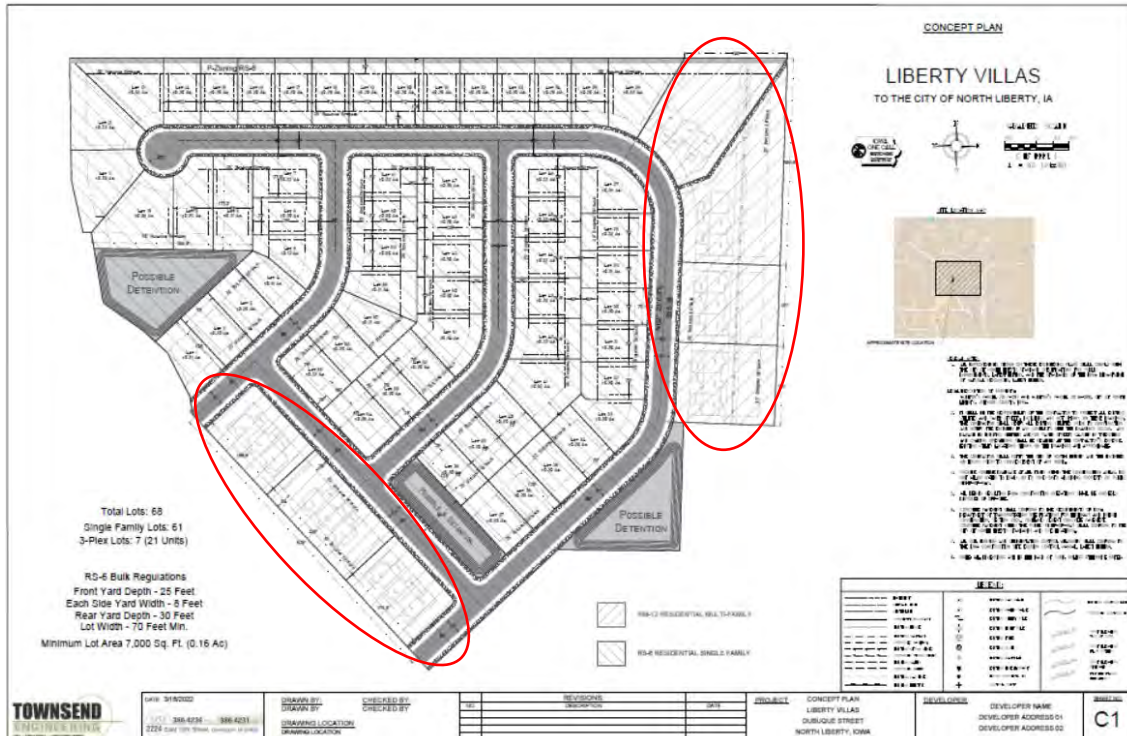
1. Current Zoning:

The property is currently zoned C-2-A Highway Commercial District and Industrial District.



2. Proposed Zoning:

The applicant is requesting a rezoning to RS-6 Single-Unit Residence District on 19.73 acres and RM-12 PAD Multi-Unit Residence District Planned Area Development on 5.91 acres.

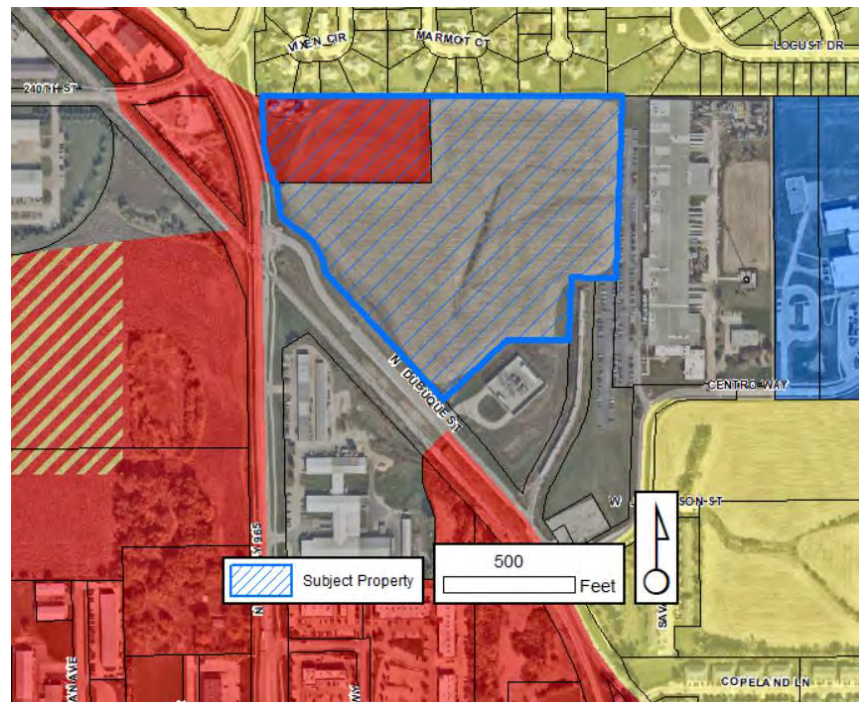


Non-binding concept plan with the townhome portion of the development circled in red.

A Planned Area Development Zone District is intended to encourage innovation and flexibility in planning the development of land so development is compatible with the site's physical and environmental characteristics. This district allows for flexibility in zone district requirements. The Planned Area Development District provides an opportunity for the development of a mixture of uses and housing types in a coordinated manner. The intent of the underlying zone district shall guide the development. It is incumbent upon the person proposing the PAD to justify the project, and any variations from the underlying zone district. A PAD is considered an amendment to the underlying zone district regulations.

The flexibility being sought is allow the townhomes within the multi-family portion of the development to construct individual curb cuts along a public street. This is not permitted in the Off-Street Parking Ordinance, as typically, there would be a townhome development double loaded on a private street with only the private street having direct access to the public street.

- 3. Comprehensive Plan Future Land Use Map Designation:** Commercial and Industrial. The proposed RS-6 and RM-12 PAD zoning would **not** be consistent with the Commercial and Industrial Land Use Designation. The City is requesting that the Future Land Use be amended to Residential.



4. Public Input:

Letters were sent to property owners within 200 feet of the subject property and notice was posted on the City's website regarding the February 14, 2022 good neighbor meeting. The meeting was held via Zoom and was very well attended with approximately 25 attendees outside of the applicant's representatives and staff.

Expressed concerns (staff commentary in italics):

1. Improving the drainage along the south side of the Fox Run Subdivision.
The applicant and staff acknowledge that drainage will need to be addressed adjacent to the north property line.
2. General questions about the final design of the subdivision, including storm water detention.
This was not able to be answered because the subdivision has not been designed.
3. Concern about the compatibility of the homes adjacent to the Fox Run Subdivision. Whether there would be a visual buffer between the two developments.
The Zoning and Subdivision Ordinance do not require a landscape buffer when residential development abuts residential development.

4. The amount of traffic on North Dubuque Street and the increased traffic caused by the proposed development. The spacing of the two entrances on North Dubuque Street.

Staff installed a covert traffic device on North Dubuque Street subsequent to the good neighbor meeting to measure northbound and southbound traffic. Average weekday southbound traffic was 1,167 vehicles per day and average weekday northbound traffic was 1,653 vehicles per day. It is anticipated that the development would generate approximately 262 average vehicle trips per day. Considering North Dubuque Street is a collector street, the amount of current traffic on North Dubuque Street is minor and the anticipated amount of traffic generated would not negatively impact roadway capacity. It's likely that the traffic is perceived as a problem due to the small window of drop-off and pick-up at Christine Grant Elementary.

5. The potential of their being an 8' wide trail (which staff advocated for) along the north end of the property and being able to accommodate drainage and adequate setbacks.

During early discussion with the applicant, staff advocated for an 8' wide path along the north property line to connect the existing path on Ranshaw Way to the existing path at the south end of the Cedar Springs Subdivision. The feasibility of this path will need to be more closely examined during review of the construction plans as accommodating drainage from the Fox Run subdivision will be required.

6. The location of the condominium development within the subdivision.
It was clarified that only the multi-family portion of the development would be a condominium. All streets within the development are proposed to be public streets constructed to City standards.

7. The style of home sites being proposed, the selling price and the applicant's experience with construction subdivisions and homes.

This was not known at the time of the neighborhood meeting.

8. If the development will offer more affordable homes for sale.

This was not known at the time of the neighborhood meeting, but based on the discussion, it appears that the units will be market rate.

9. How the proposed development would impact the capacity of Christine Grant Elementary.

It is staff's opinion that the proposed development would not cause Christine Grant to exceed design capacity. However, staff has an inquiry with the Iowa City Community School District and will provide an update once ICCSD responds.

There were a lot of questions regarding the design of the subdivision when this in the initial phase of the development process. Staff is cognizant that "fitting the last piece of the puzzle" is important and the feedback received will need to be considered when the subdivision is designed.

Considering there were so many questions and some confusion, a second good neighbor meeting was held in person and via Zoom on March 22, 2022. Notice was mailed to surrounding properties owners and notice was posted on the City's website regarding the good neighbor meeting. Additionally, staff emailed notice of the meeting to the registrants of the February 14, 2002 meeting. The locations of the RM-12 zoning was

revised. A few people attended in person and approximately five people attended online. Staff did not observe any new concerns.

To date, staff has received one verbal objection the request from the property owner at 80 Marmot Court. The property verbalized that they would rather have no development or industrial development or behind their property.

5. Analysis of the Request:

The request for residential zoning is considered much less intense than commercial and industrial zoning. This is often referred to as a downzoning. It is staff's opinion that based on city development over the past 30+ years that residential development is much more appropriate in location. Please refer to the staff's analysis of the Comprehensive Plan Future Land Use Amendment.

Fire prevention requires a minimum of two accesses from a public street when there are 30 or more residential units. The entrances would be spaced to the maximum extent while maintaining adequate distance from the curve at the north end of North Dubuque Street.

As mentioned, the Planned Area Development is being sought is allow the townhomes within the multi-family portion of the development to construct individual curb cuts along a public street. This is not permitted in the Off-Street Parking Ordinance as typically there would be a townhome development double loaded on a private street with only the private street having direct access to the public street. Below is the Dahnovan Estates Subdivision, in which Ronald Way and Lilly Lane are private streets and North Liberty Road and East Tartan Drive are public streets.



In a typical townhome development, such as Dahnovan Estates, there would be several housing units on either side of the street. As such, it would not safe to have individual driveway accesses on a public street.

However, considering the configuration of the property and the limited locations of the proposed townhomes, staff in favor of allowing driveway access on a public street.

Section 165.09 of the Zoning Ordinance sets for the approval standards for zoning maps amendments.

D. Approval Standards. The Planning Commission recommendation and the City Council decision on any zoning text or map amendment is a matter of legislative discretion that is not controlled by any particular standard. However, in making their recommendation and decision, the Plan Commission and the City Council must consider the following standards. The approval of amendments is based on a balancing of these standards (staff commentary in italics).

(1) Map Amendments.

(a) The consistency of the proposed amendment with the Comprehensive Plan and any adopted land use policies.

It is staff's opinion that the zoning map amendment would achieve consistency with the Comprehensive Plan if the Future Land Use Map if amended to Residential. As mentioned, it is staff's opinion that the Residential Land Use is more appropriate in this location than Commercial and/or Industrial.

(b) The compatibility with the zoning of nearby property.

It is staff's opinion that the proposed zonings would be more compatible with nearby property than Commercial and/or Industrial.

(c) The compatibility with established neighborhood character.

It is staff's opinion that the proposed zonings would be compatible with established neighborhood character.

(d) The extent to which the proposed amendment promotes the public health, safety, and welfare of the City.

It is staff's opinion that the proposed zonings as residential would promote the public health, safety, and welfare of the City.

(e) The suitability of the property for the purposes for which it is presently zoned, i.e. the feasibility of developing the property in question for one or more of the uses permitted under the existing zoning classification.

It is staff's opinion that the property, as presently zoned, would be suitable to be developed with commercial and industrial development. However, it is staff's opinion that residential development would be more compatible with the zoning of nearby property and would be more compatible with the established neighborhood character.

(f) The extent to which the proposed amendment creates nonconformities.

This is not applicable.

6. Additional Considerations:

The City will require an 8' wide path along the east side of North Dubuque Street. The utility substations deferred installation of the 8' wide path until such as this property developed. This would development would allow for a key connection to the City multi-use path network.

Below is the residential unit count in North Liberty, which does not include the proposed development.

SFR	2FR	MFR	MFR w/Comm	MH	Total
3224	1598	2896	230	470	8418
38.30%	18.98%	34.40%	2.73%	5.58%	

As part of the Comprehensive Plan update data collection, staff documented the housing construction over the past 11 years:

SFR	2FR	TH	MF	MFR w/Comm	MH	Total
1,127	568	513	602	72	0*	8418
39.88%	20.10%	18.15%	21.30%	2.55%		

* Not included as there haven't been any manufactured housing park expansions, just replacement units.

7. Recommendation:

Finding:

1. The proposed zonings would achieve consistency with Section 165.09 of the Zoning Ordinance if the Comprehensive Plan Future Land Use Map is amended to Residential.

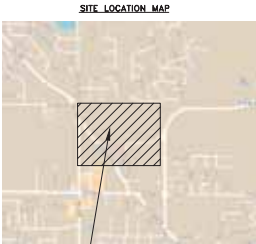
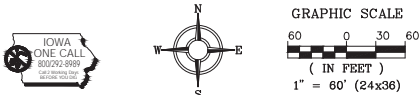
Staff recommends the Planning Commission accept the listed finding and forward the request of Daniel & Rhonda Bernacki Revocable Trust for a zoning map amendment (rezoning) on 25.64 acres, more or less, on property located on the east side of North Dubuque Street approximately 185' south of Scales Bend Road from C-2-A Highway Commercial District and I-1 Light Industrial District to RS-6 Single-Unit Residence District (19.73 acres) and RM-12 PAD Multi-Unit Residence District Planned Area Development (5.91 acres) to the City Council with a recommendation for approval.

Suggested motion:

I move that the Planning Commission accept the listed finding and forward the zoning map amendment to the City Council with a recommendation for approval.

CONCEPT PLAN

LIBERTY VILLAS
TO THE CITY OF NORTH LIBERTY, IA



- GENERAL NOTES**
1. ALL IMPROVEMENTS SHOWN ON THESE ENGINEERING PLANS SHALL COMPLY WITH THE CITY OF NORTH LIBERTY STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENTS, LATEST EDITION, AND THE STANDARDS OF THE IOWA DEPARTMENT OF NATURAL RESOURCES, LATEST EDITION.
 2. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES AND PAVED STREETS, INCLUDING ANY NOT SHOWN ON THESE DRAWINGS. THE CONTRACTOR SHALL VERIFY ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER IF ANY CONFLICTS WITH THE DRAWINGS OCCUR. ANY DAMAGE TO EXISTING UTILITIES AND/OR PAVED STREETS CAUSED BY TRENCHING AND GRADING OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. EXISTING UTILITY LOCATIONS SHOWN ON THE DRAWINGS ARE APPROXIMATE.
 3. THE CONTRACTOR SHALL NOTIFY THE CITY OF NORTH LIBERTY AND THE ENGINEER 48 HOURS PRIOR TO COMMENCEMENT OF ANY WORK.
 4. PROVIDE POSITIVE DRAINAGE AT ALL TIMES WITHIN THE CONSTRUCTION AREAS. DO NOT ALLOW WATER TO DRAIN OR TO POND ONTO ADJOINING PROPERTY OR PUBLIC RIGHT-OF-WAY.
 5. ALL DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE PROPERLY DISPOSED OF OFF-SITE.
 6. CONCRETE PAVEMENT SHALL CONFORM TO THE REQUIREMENTS OF IOWA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS FOR HIGHWAYS AND BRIDGE CONSTRUCTION, SECTION 2301, PORTLAND CEMENT CONCRETE PAVEMENT. CONCRETE PAVEMENT WITHIN THE PUBLIC RIGHT-OF-WAY SHALL CONFORM TO THE CITY OF NORTH LIBERTY STANDARDS AND SPECIFICATIONS.
 7. ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL CONFORM TO THE IOWA CONSTRUCTION SITE EROSION CONTROL MANUAL, LATEST EDITION.
 8. NOTE: ALL DIMENSIONS ARE TO THE BACK OF CURB, UNLESS OTHERWISE NOTED.

LEGEND:			
	EXISTENT		EXISTING GAS VALVE
	SETBACK LINE		EXISTING WATER VALVE
	CENTERLINE		EXISTING UTILITY POLE
	PROPERTY BOUNDARY		EXISTING LIGHT POLE
	EXISTING FENCE		EXISTING TREE
	EXISTING SANITARY		EXISTING BUSH
	PROPOSED SANITARY		EXISTING MANHOLE
	EXISTING STORM SEWER		EXISTING FIRE HYDRANT
	PROPOSED STORM SEWER		FOUND PROPERTY PIN
	EXISTING WATER		CONTROL POINT
	PROPOSED WATER		
	EXISTING GAS LINE		
	EXISTING ELECTRIC		
	RM-12 RESIDENTIAL MULTI-FAMILY		EXISTING CONTOUR LINE
	RS-6 RESIDENTIAL SINGLE FAMILY		PROPOSED CONTOUR LINE

Total Lots: 68
Single Family Lots: 61
3-Plex Lots: 7 (21 Units)

RS-6 Bulk Regulations
Front Yard Depth - 25 Feet
Each Side Yard Width - 8 Feet
Rear Yard Depth - 30 Feet
Lot Width - 70 Feet Min.

Minimum Lot Area 7,000 Sq. Ft. (0.16 Ac)



DATE: 3/16/2022
386 4236 386 4231
2224 East 12th Street, Des Moines, IA 50319

DRAWN BY: _____
CHECKED BY: _____
DRAWING LOCATION: _____
DRAWING LOCATION: _____

NO.	REVISIONS: DESCRIPTION	DATE

PROJECT: CONCEPT PLAN
LIBERTY VILLAS
DUBUQUE STREET
NORTH LIBERTY, IOWA

DEVELOPER: _____
DEVELOPER NAME: _____
DEVELOPER ADDRESS 01: _____
DEVELOPER ADDRESS 02: _____

SHEET NO.:
C1



1/4" = 1' FRONT

Elevation 6

SHEET:

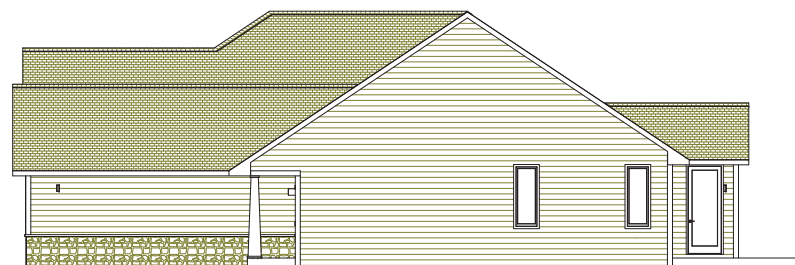
1



1/4" = 1' REAR



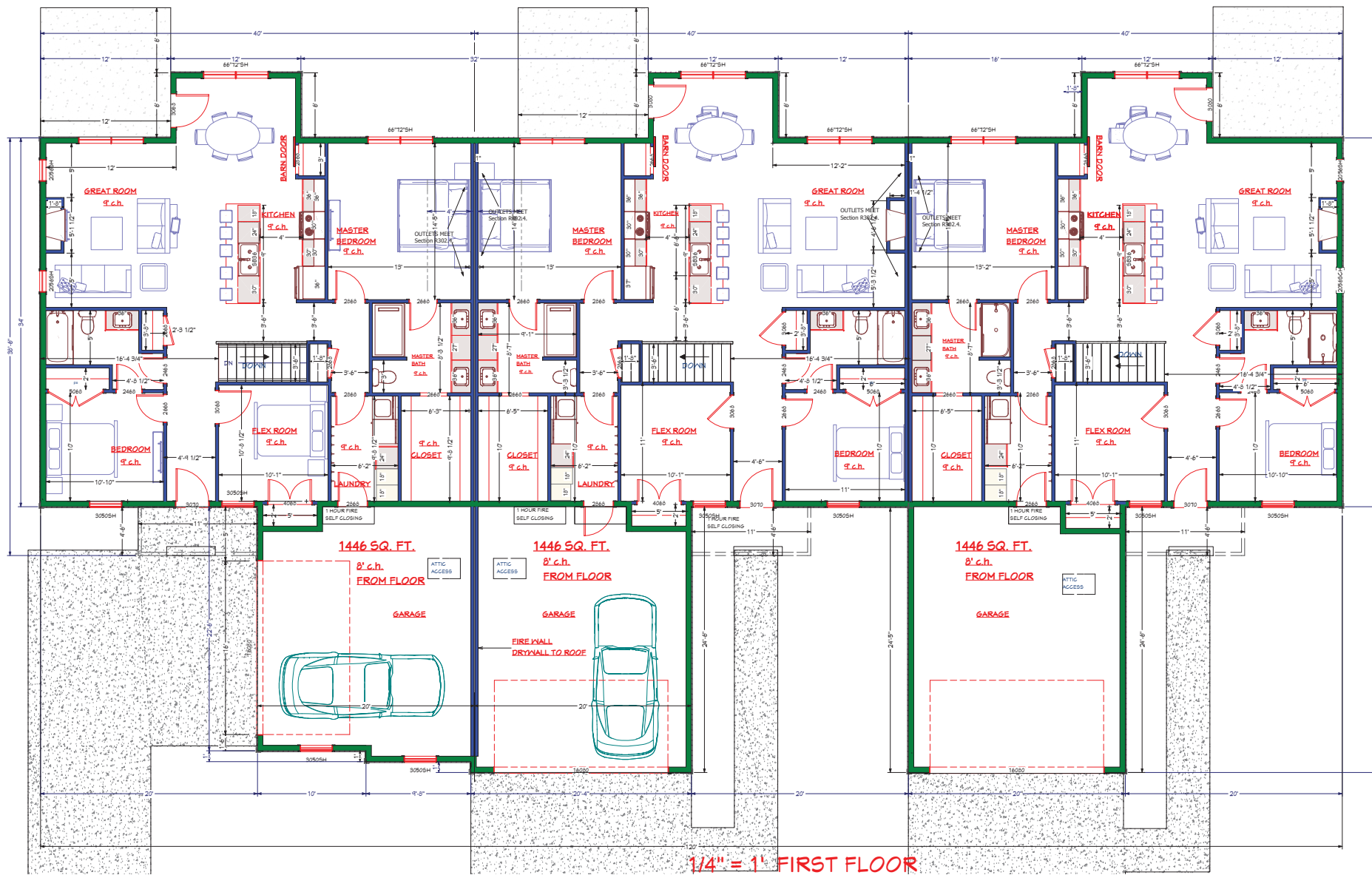
3/16" = 1' LEFT

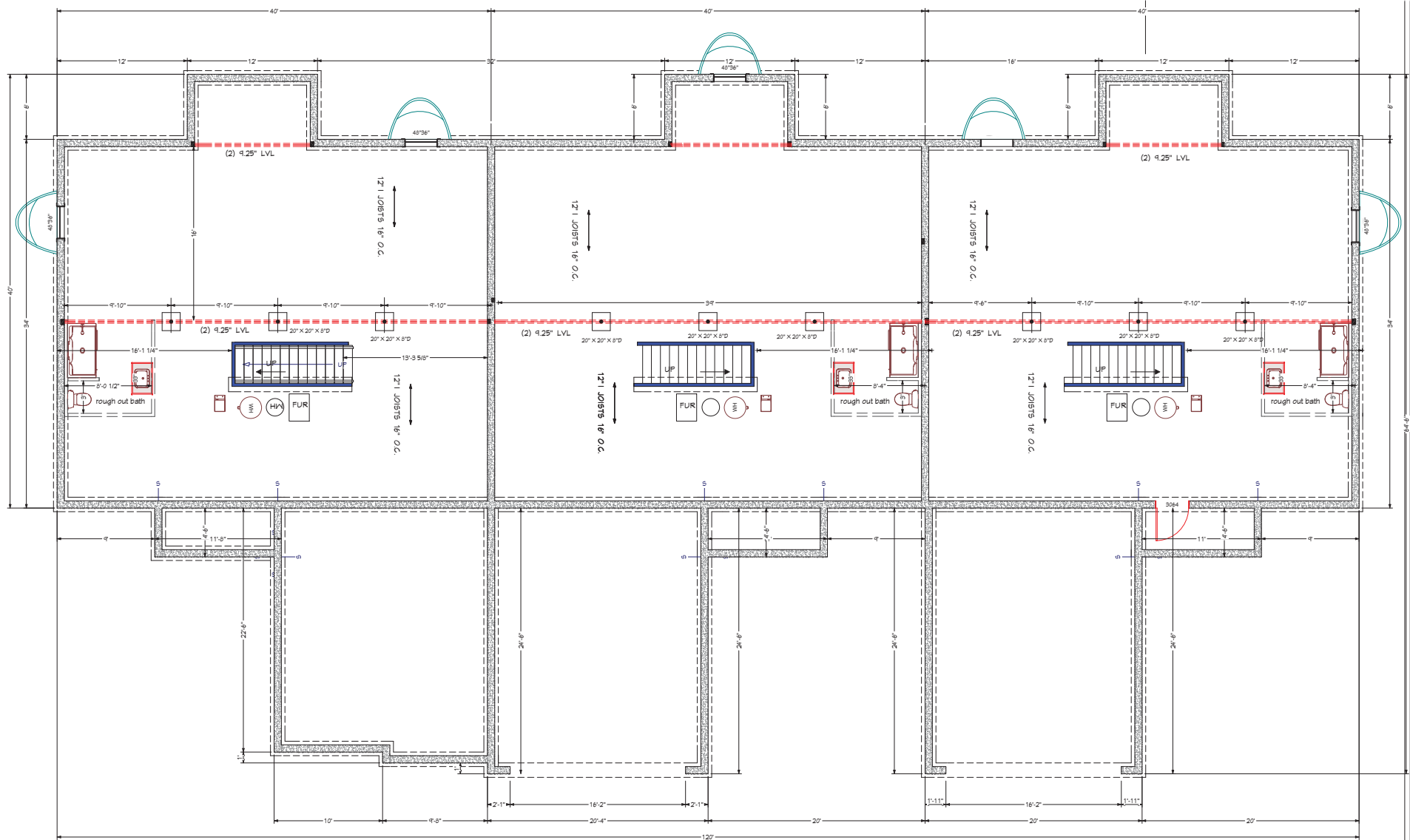


3/16" = 1' RIGHT

SHEET:

2

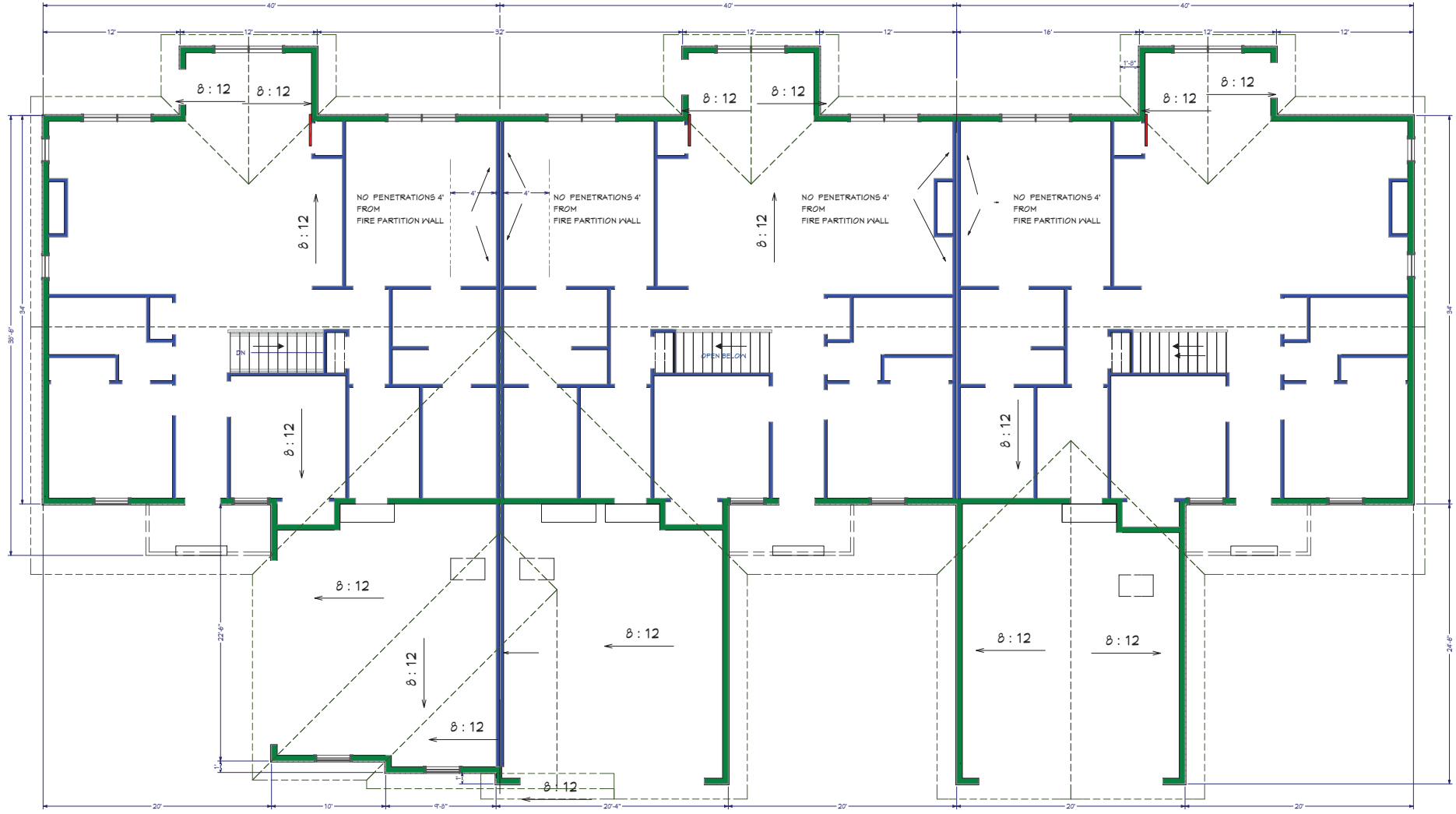




1/4" = 1' BASEMENT

SHEET:

4

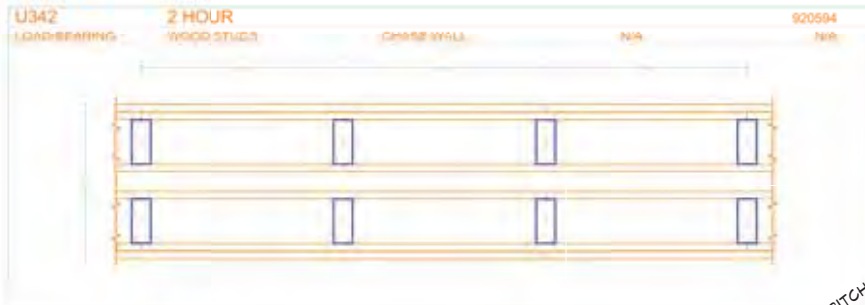


1/4" = 1' ROOF

SHEET:

5

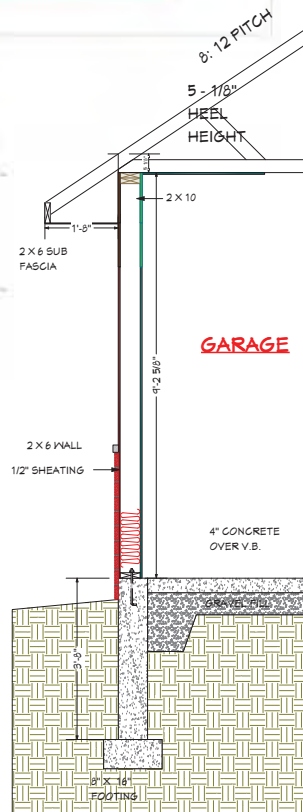
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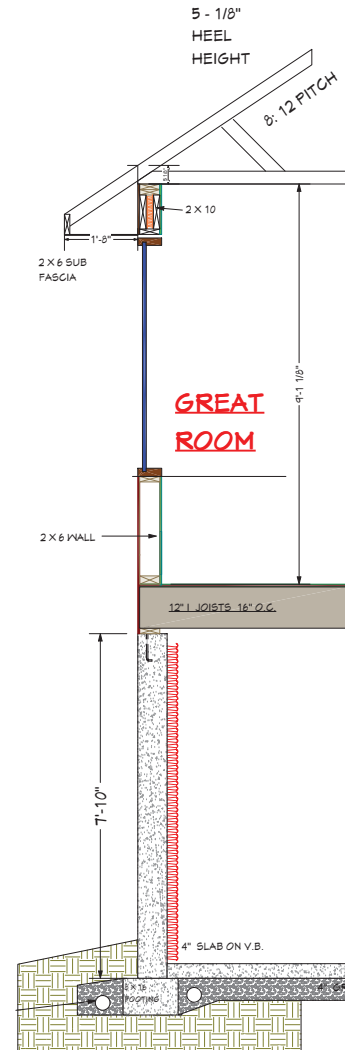
FIRE RATING: 2 HOUR
STC: N/A
SOUND TEST: N/A
SYSTEM THICKNESS: 12-1/4"

GYPSUM BOARD: TWO LAYERS OF 5/8 IN. THICK GYPSUM BOARD APPLIED HORIZONTALLY OR VERTICALLY.
WOOD STUDS: 2 IN. X 4 IN. WOOD STUDS SPACED MAX. 16 IN. O.C., CROSS BRACED AT MID-HEIGHT AND FIRESTOPPED AT TOP AND BOTTOM OF WALL.
GYPSUM BOARD: 5/8 IN. THICK GYPSUM BOARD APPLIED HORIZONTALLY OR VERTICALLY.
AIR CAVITY: 1-1/2 IN. THICK AIR SPACE
GYPSUM BOARD: 5/8 IN. THICK GYPSUM BOARD APPLIED HORIZONTALLY OR VERTICALLY.
WOOD STUDS: 2 IN. X 4 IN. WOOD STUDS SPACED MAX. 16 IN. O.C., CROSS BRACED AT MID-HEIGHT AND FIRESTOPPED AT TOP AND BOTTOM OF WALL.
GYPSUM BOARD: TWO LAYERS OF 5/8 IN. THICK GYPSUM BOARD APPLIED HORIZONTALLY OR VERTICALLY.

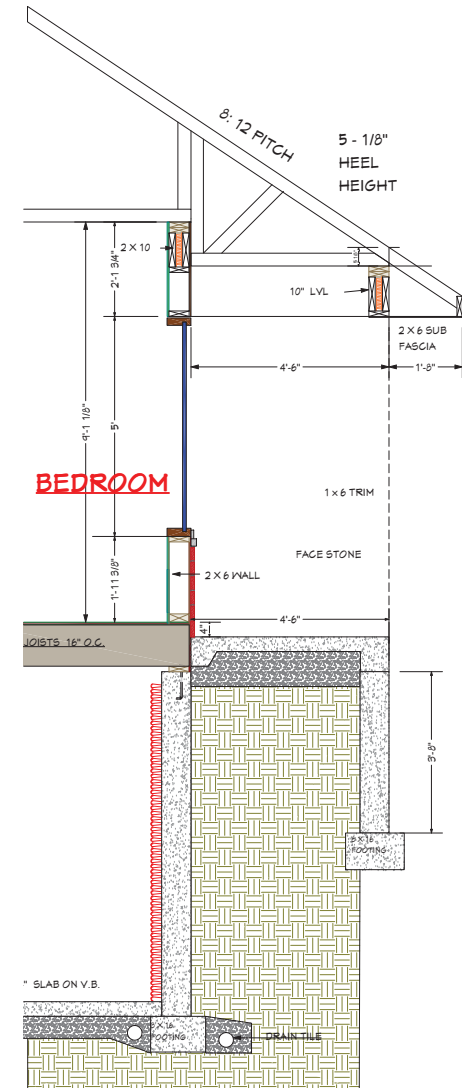
UL U342
2 HOUR FIRE
PARTION



3/4" = 1'
GARAGE WALL



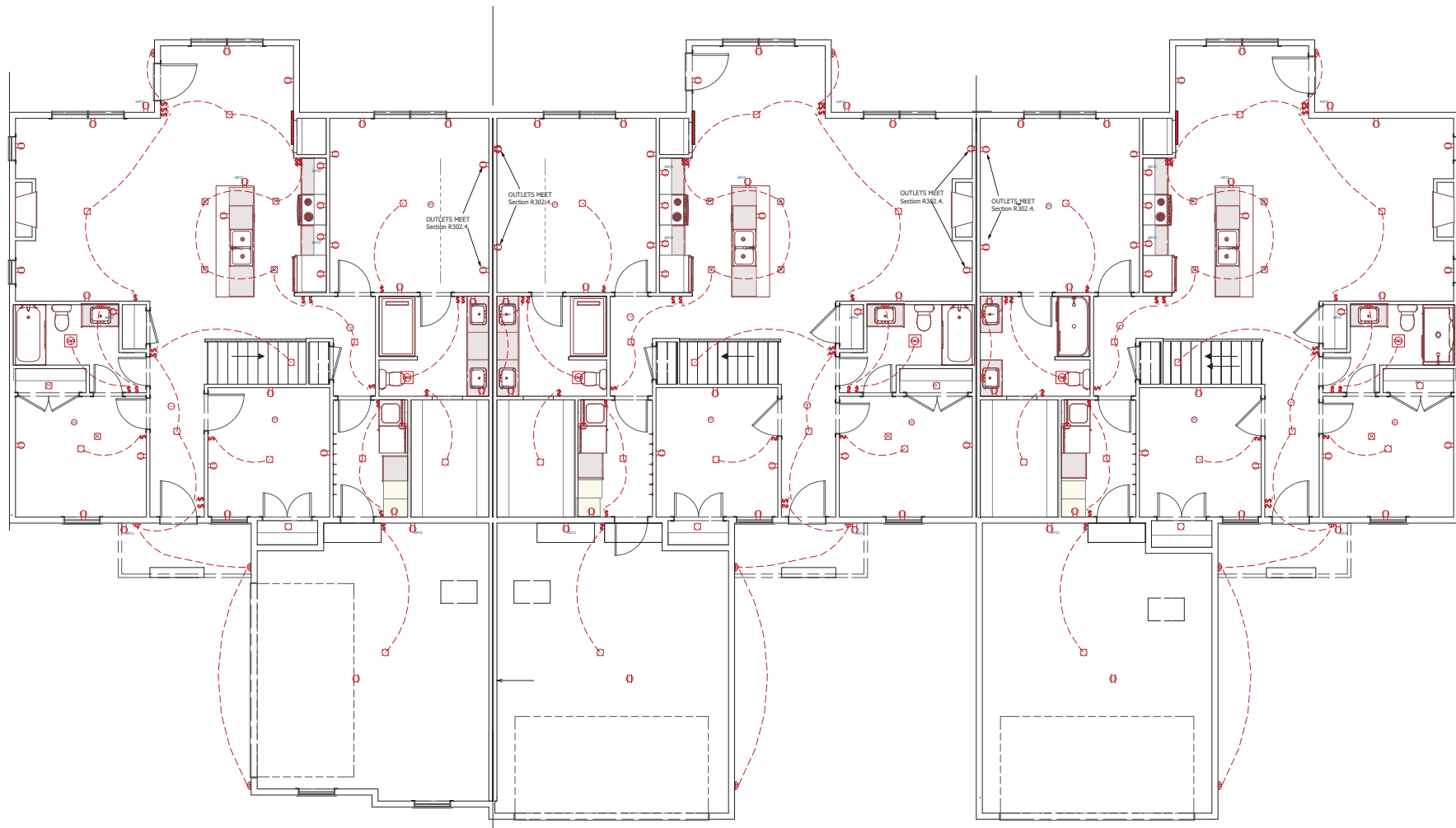
3/4" = 1'
GREAT ROOM WALL



3/4" = 1' BEDROOM WALL

SHEET:

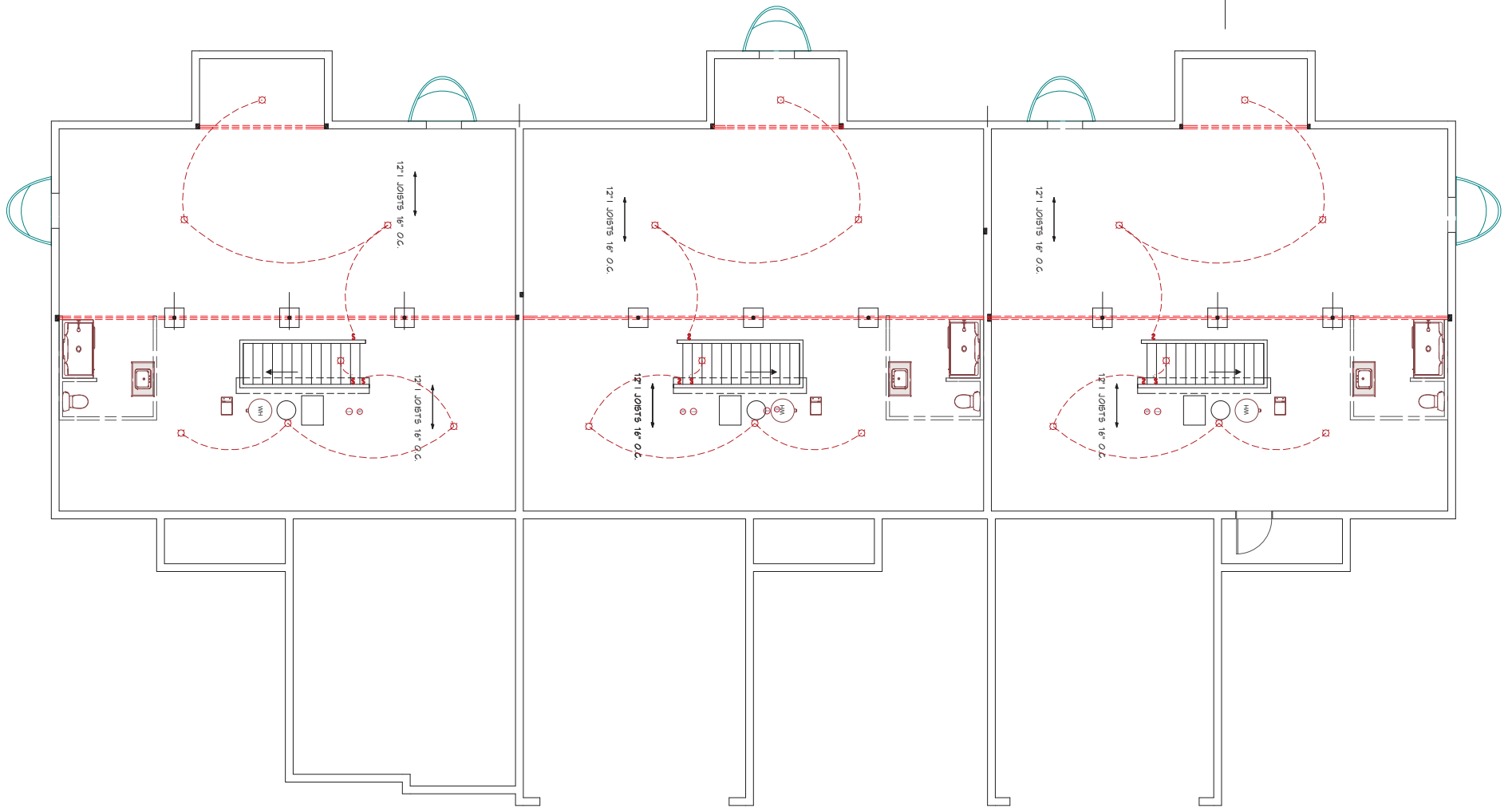
6



1st Floor ELECTRIC
1/4" = 1'

SHEET:

7



ELECTRIC BASEMENT
1/4" = 1'

For Project:	N Dubuque St Project				
Project Notes:					
Location/Name:	Outgoing				
Report Generated:	2/28/2022	11:39			
Speed Intervals	1 MPH				
Time Intervals	Instant				
Traffic Report From	2/16/2022	10:00:00	through	2/28/2022	12:59:59
85th Percentile Speed	40 MPH				
85th Percentile Vehicles	12176				
Max Speed	74 MPH	on	2/26/2022	13:54:46	
Total Vehicles	14325				
AADT:	1181				

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1223	1108
AM Peak	08:00 237	177
PM Peak	03:00 107	95

Speed

Speed Limit:	45
85th Percentile Speed:	40
Average Speed:	34.94

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	35	20	51	39	55	33	43
% over limit	1.8	1.7	2.2	1.5	1.9	2.0	2.6
Avg Speeder	48.6	48.6	48.1	48.6	48.3	49.8	48.3

Class Counts

	Number	%
VEH_SM	59	0.4
VEH_MED	13872	96.8
VEH_LG	394	2.8
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Weekly Counts
N Dubuque St Project

from Wed-Feb-16-2022-10-00-AM to Mon-Feb-28-2022-12-59-PM

	2/14/2022	to	2/20/2022							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	2/14/2022	2/15/2022	2/16/2022	2/17/2022	2/18/2022	2/19/2022	2/20/2022	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	5	12	16	16	8.5	16	33.65
1 - 2	*	*	*	1	6	8	6	3.5	7	28
2 - 3	*	*	*	3	1	1	2	2	1.5	36.5
3 - 4	*	*	*	3	4	5	2	3.5	3.5	36
4 - 5	*	*	*	4	2	2	3	3	2.5	37
5 - 6	*	*	*	6	6	4	0	6	2	36
6 - 7	*	*	*	24	25	8	9	24.5	8.5	38.3
7 - 8	*	*	*	99	102	26	14	100.5	20	38.35
8 - 9	*	*	*	242	246	20	28	244	24	37.3
9 - 10	*	*	*	135	107	38	42	121	40	41.1
10 - 11	*	*	9	62	53	48	43	41.33	45.5	42.83
11 - 12	*	*	63	60	59	38	67	60.67	52.5	38.27
12 - 13	*	*	51	46	64	46	51	53.67	48.5	38.07
13 - 14	*	*	66	50	71	54	64	62.33	59	40.97
14 - 15	*	*	57	90	59	56	66	68.67	61	38.67
15 - 16	*	*	92	88	120	63	70	100	66.5	39.5
16 - 17	*	*	66	68	102	54	82	78.67	68	39.53
17 - 18	*	*	86	136	101	53	65	107.67	59	39.63
18 - 19	*	*	109	100	123	70	85	110.67	77.5	39
19 - 20	*	*	113	58	71	38	40	80.67	39	39.17
20 - 21	*	*	65	45	75	28	29	61.67	28.5	37.5
21 - 22	*	*	52	32	37	25	22	40.33	23.5	36
22 - 23	*	*	18	14	26	42	19	19.33	30.5	35.67
23 - 24	*	*	29	28	20	24	25	25.67	24.5	35.93
Totals	0	0	876	1399	1492	767	850			
% of Total	0%	0%	16.27%	25.98%	27.71%	14.25%	15.79%			

Outgoing Weekly Counts
N Dubuque St Project

from Wed-Feb-16-2022-10-00-AM to Mon-Feb-28-2022-12-59-PM

	2/21/2022	to	2/27/2022							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	2/21/2022	2/22/2022	2/23/2022	2/24/2022	2/25/2022	2/26/2022	2/27/2022	Day Avg	Avg	Avg Speed
0 - 1	4	4	5	7	4	14	17	4.8	15.5	33.2
1 - 2	2	4	3	4	3	6	7	3.2	6.5	34.1
2 - 3	0	2	1	1	3	1	1	1.4	1	34
3 - 4	4	5	3	1	4	1	4	3.4	2.5	34.2
4 - 5	3	4	0	2	4	2	2	2.6	2	33.25
5 - 6	6	8	4	10	6	1	3	6.8	2	37.3
6 - 7	27	19	22	27	13	5	5	21.6	5	38.2
7 - 8	90	85	99	100	79	22	11	90.6	16.5	38.26
8 - 9	212	229	269	233	208	40	19	230.2	29.5	36.14
9 - 10	130	118	123	144	129	45	38	128.8	41.5	38.7
10 - 11	77	66	71	68	62	44	51	68.8	47.5	39.08
11 - 12	43	33	46	44	50	43	57	43.2	50	39.18
12 - 13	54	36	65	58	39	69	49	50.4	59	38.36
13 - 14	76	51	59	52	62	62	60	60	61	40.02
14 - 15	60	37	55	94	68	68	60	62.8	64	38.94
15 - 16	117	118	117	84	106	50	72	108.4	61	38.16
16 - 17	70	65	77	57	83	60	83	70.4	71.5	39.26
17 - 18	102	86	85	89	116	73	62	95.6	67.5	37.86
18 - 19	92	86	130	82	121	81	71	102.2	76	38.78
19 - 20	89	51	80	46	60	49	57	65.2	53	37.28
20 - 21	35	30	42	27	54	33	27	37.6	30	37.74
21 - 22	19	14	39	22	19	27	25	22.6	26	35.24
22 - 23	12	19	14	10	21	21	16	15.2	18.5	37.28
23 - 24	28	27	33	26	33	26	30	29.4	28	37.2
Totals	1352	1197	1442	1288	1347	843	827			
% of Total	16.3%	14.43%	17.38%	15.53%	16.24%	10.16%	9.97%			

For Project:	N Dubuque St Project					
Project Notes:						
Location/Name:	Incoming					
Report Generated:	2/28/2022	11:39				
Speed Intervals	1 MPH					
Time Intervals	Instant					
Traffic Report From	2/16/2022	10:00:00	through	2/28/2022	12:59:59	
85th Percentile Speed	43 MPH					
85th Percentile Vehicles	14634					
Max Speed	69 MPH	on	2/19/2022	10:08:06		
Total Vehicles	17217					
AADT:	1419					

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1492	1337
AM Peak	08:00 211	156
PM Peak	04:00 205	166

Speed

Speed Limit:	45
85th Percentile Speed:	43
Average Speed:	38.18

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	175	30	201	165	219	185	187
% over limit	8.2	2.0	6.5	5.1	6.3	9.7	9.9
Avg Speeder	48.0	47.1	48.4	48.2	48.2	48.9	48.0

Class Counts

	Number	%
VEH_SM	310	1.8
VEH_MED	16409	95.3
VEH_LG	498	2.9
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Weekly Counts
N Dubuque St Project

from Wed-Feb-16-2022-10-00-AM to Mon-Feb-28-2022-12-59-PM

	2/14/2022	to	2/20/2022							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	2/14/2022	2/15/2022	2/16/2022	2/17/2022	2/18/2022	2/19/2022	2/20/2022	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	22	26	39	30	24	34.5	38.5
1 - 2	*	*	*	4	3	20	10	3.5	15	37.5
2 - 3	*	*	*	6	0	9	9	3	9	33
3 - 4	*	*	*	0	3	2	1	1.5	1.5	39
4 - 5	*	*	*	6	5	3	4	5.5	3.5	38
5 - 6	*	*	*	7	3	3	2	5	2.5	45.25
6 - 7	*	*	*	8	8	3	6	8	4.5	39.5
7 - 8	*	*	*	51	40	0	4	45.5	2	41.7
8 - 9	*	*	*	212	216	22	18	214	20	40.65
9 - 10	*	*	*	77	80	31	30	78.5	30.5	42.5
10 - 11	*	*	7	52	36	56	42	31.67	49	42
11 - 12	*	*	46	38	46	43	54	43.33	48.5	42.43
12 - 13	*	*	62	53	77	49	71	64	60	44.63
13 - 14	*	*	79	61	75	64	87	71.67	75.5	43.93
14 - 15	*	*	59	67	69	74	80	65	77	43.4
15 - 16	*	*	86	156	105	62	80	115.67	71	43.6
16 - 17	*	*	206	181	229	64	82	205.33	73	42.8
17 - 18	*	*	217	181	228	75	82	208.67	78.5	42.93
18 - 19	*	*	199	187	184	69	78	190	73.5	42.07
19 - 20	*	*	130	83	102	67	75	105	71	41.27
20 - 21	*	*	76	58	60	52	48	64.67	50	39.93
21 - 22	*	*	94	69	71	43	36	78	39.5	39.17
22 - 23	*	*	42	36	33	32	23	37	27.5	39.67
23 - 24	*	*	21	24	55	26	12	33.33	19	37.6
Totals	0	0	1324	1639	1754	908	964			
% of Total	0%	0%	20.09%	24.87%	26.62%	13.78%	14.63%			

Incoming Weekly Counts
N Dubuque St Project

from Wed-Feb-16-2022-10-00-AM to Mon-Feb-28-2022-12-59-PM

	2/21/2022	to	2/27/2022							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	2/21/2022	2/22/2022	2/23/2022	2/24/2022	2/25/2022	2/26/2022	2/27/2022	Day Avg	Avg	Avg Speed
0 - 1	4	24	22	32	29	46	28	22.2	37	37.86
1 - 2	2	4	8	5	3	10	13	4.4	11.5	37.6
2 - 3	1	3	5	1	2	7	7	2.4	7	36.4
3 - 4	0	2	0	5	1	4	4	1.6	4	40
4 - 5	2	6	0	5	12	7	5	5	6	41.75
5 - 6	7	10	7	6	8	4	3	7.6	3.5	41.4
6 - 7	8	11	11	9	11	7	3	10	5	41.08
7 - 8	43	37	42	51	43	11	5	43.2	8	41.56
8 - 9	219	200	213	205	207	20	14	208.8	17	39.94
9 - 10	83	75	84	86	67	43	35	79	39	41.94
10 - 11	48	27	48	47	40	65	40	42	52.5	42.36
11 - 12	58	42	65	31	50	52	63	49.2	57.5	42.48
12 - 13	60	41	70	64	64	50	65	59.8	57.5	43.7
13 - 14	56	68	70	59	74	58	83	65.4	70.5	44.32
14 - 15	60	62	60	60	66	73	67	61.6	70	44
15 - 16	73	76	74	165	92	58	77	96	67.5	43.14
16 - 17	207	199	220	177	232	65	67	207	66	41.86
17 - 18	179	172	231	156	214	86	80	190.4	83	41.68
18 - 19	217	169	189	192	219	94	68	197.2	81	41.74
19 - 20	90	83	120	77	110	72	85	96	78.5	40.56
20 - 21	52	63	69	53	51	52	53	57.6	52.5	39.66
21 - 22	75	84	92	45	40	41	27	67.2	34	39.14
22 - 23	40	33	43	25	51	32	16	38.4	24	39.82
23 - 24	17	14	19	15	52	42	21	23.4	31.5	38.32
Totals	1601	1505	1762	1571	1738	999	929			
% of Total	15.84%	14.89%	17.44%	15.55%	17.2%	9.89%	9.19%			



To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of Archie's Rent A Ride, LLC to approve a Site Plan for a minor vehicle repair facility and related site improvements on approximately 1.5 acres of property located at 1040 Liberty Way.**

North Liberty City staff has reviewed the subject submission, and offer comments presented in this memo. The staff review team includes the following personnel:

Ryan Heiar, City Administrator
Tracey Mulcahey, Assistant City Administrator
Grant Lientz, City Attorney
Tom Palmer, City Building Official
Kevin Trom, City Engineer
Ryan Rusnak, Planning Director

1. Request Summary:

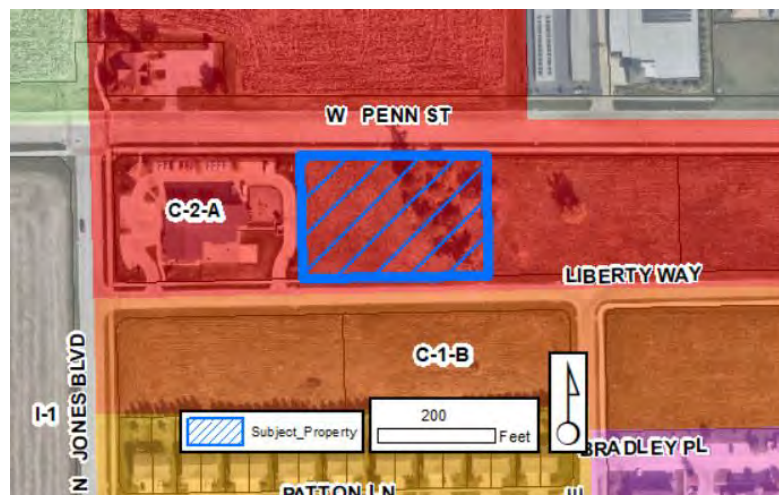
The site plan proposes to construct a 5,875 square foot minor vehicle repair facility and related infrastructure.

2. Existing Zoning:

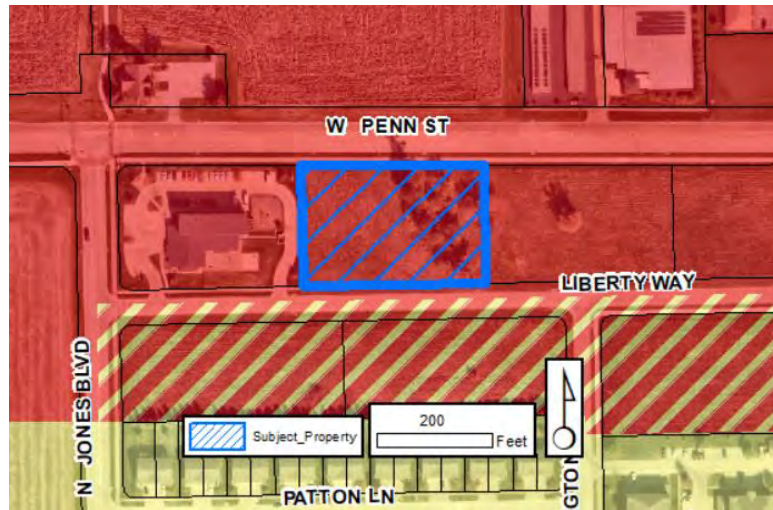
The property is currently zoned C-2-A Highway Commercial District. "Vehicle repair, minor" is listed as a permitted use in the C-2-A District.

C-2-A District Description:

C-2-A Highway Commercial District. The C-2-A District is intended to provide for those commercial uses which may take particular advantage of a highway location and/or due to size or other nuisance constraints may be incompatible with the predominantly retail uses permitted in the C-1-A and C-1-B Commercial Districts, and whose service area is not confined to any one neighborhood or community.



3. **Comprehensive Plan Future Land Use Map Designation:** Commercial. The C-2-A Zoning is consistent with the Commercial Land Use Designation.



The North Liberty Comprehensive Plan articulates the following regarding commercial uses:

The highest densities of more locally-oriented commercial uses are generally planned along Penn Street and along Highway 965. Interstate-related large scale office and mixed-use developments are planned for the Kansas Avenue corridor, especially between Kansas and I-380 and near the planned Forevergreen Road interchange. High visibility from the I-380 corridor presents an excellent opportunity to introduce high-quality office and commercial uses in this “front door” area that may serve both the external demand associated with the I-380 proximity and access and the internal demand generated by substantial recent and planned residential development in the area.

Relevant Comprehensive Plan Policies Related to Land Use:

- Establish and maintain an advantageous property tax situation and pursue a strengthened and sound tax base through a diversification of land uses, including commercial and industrial development, as well as a variety of residential options.

4. Public Input:

No public comments have been received.

5. Approval Standards:

Section 165.05(2) of the North Liberty Code of Ordinances entitled, “Preliminary Site Review” sets forth the approval standards (Ordinance language in *italics* and staff analysis in **bold**).

- *Date, north arrow and graphic scale.* **Provided on Sheet C0.00.**
- *The property owner’s name and description of proposed development.* **Provided on Sheet C0.00.**
- *A vicinity sketch showing the location of the property and other properties within 1,000 feet of it.* **Provided on Sheet C0.00.**

- *Property boundary lines, dimensions, and total area.* **Provided on Sheets C0.00 and C2.00.**
- *Contour lines at intervals of not more than five feet, City datum. If substantial topographic change is proposed, the existing topography shall be illustrated on a separate map and the proposed finished topography shown on the site plan.* **Provided on Sheet C1.00.**
- *The location of existing streets, sidewalks, easements, utilities, drainage courses.* **Provided on Sheet C4.00.**
- *The total square feet of building floor area, both individually and collectively.* **Provided on Sheet C0.00.**
- *All structures and major features shall be fully dimensioned including distance between structures, distance between driveways, parking areas, property lines and building height.* **Provided on Sheet C2.00. Building height is not provided, but it's a one-story building so building height is not a concern.**
- *Off-street parking areas, ingress and egress to the property, number of parking spaces proposed, number of parking spaces required by this code and type of surfacing.* **Provided on Sheets C0.00 and C2.00.**
- *Pedestrian walkways with special consideration given to pedestrian safety.* **Provided on Sheet C2.00. There is a dedicated pedestrian walkway from the West Penn Street sidewalk to the front entrance of the building.**
- *Trash and refuse enclosures.* **Provided on Sheets C2.00 and C5.00.**
- *The general drainage pattern and location of storm water detention features.* **Provided on Sheet C4.00.**
- *The general location, type and size of landscaping and ground cover illustrated in color perspective.* **Provided on landscape rendering.**
- *A rendering, elevation or photo of the proposed development.* **Provided on the architectural rendering.**

North Liberty Code of Ordinances Section 169.10 entitled, "Design Standards" sets forth certain design standards (applicable Ordinance language in italics and staff analysis in bold).

Requirements for All Districts.

- Building design shall be visually harmonious and compatible with the neighborhood character.
- Buildings located on property with double frontages shall have similar wall design facing both streets.
- Except for RS RD, R-MH and ID districts, color schemes shall be primarily based on earth tones. Earth tone colors include colors from the palette of browns, tans, greys, greens, and red. Earth tone colors shall be flat or muted. Building trim and accent areas may feature non-earth tone and brighter colors. In any district, the use of high intensity colors, neon or fluorescent color and neon tubing is prohibited.
- Except in the R-MH district, a minimum roof pitch of 5:12 shall apply to gable, hip, or shed roofs and there shall be a minimum roof overhang at the eaves of 12 inches. This does not apply to portions of a roof that are separate from the structure's primary

roof. Metal roofs shall not be corrugated or similar appearance. The color of the roof shall be visually harmonious and compatible with the building color scheme.

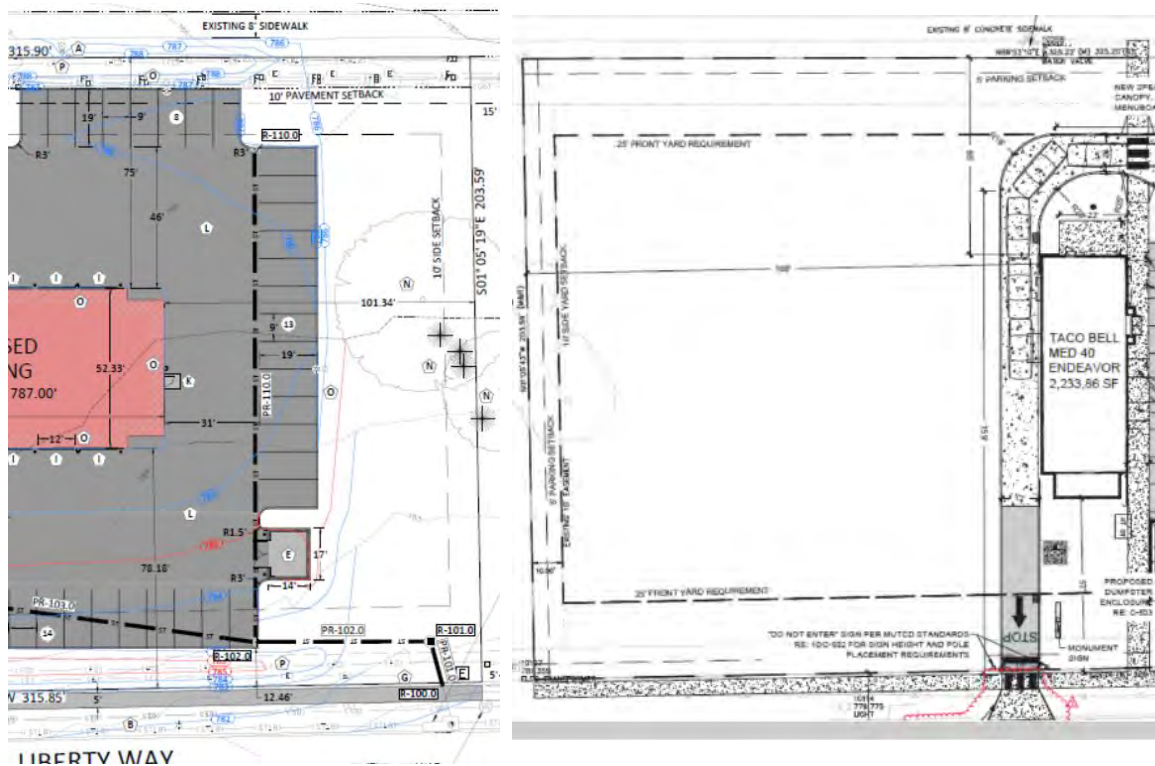
Requirements for Development in Commercial Districts.

- *Design. To achieve appealing aesthetic design through high quality architecture and construction, with attention to placement, relationship, and orientation of structures and amenities to provide both internal cohesiveness and compatibility with surrounding uses.*
- *Walkability. To achieve overall development patterns that encourage walking and reduce dependence on the automobile to travel from one business to another, and so reduces the dominance of the automobile within the development.*
- *Parking Areas. Parking areas shall consist of areas that are aesthetically pleasing, landscaped to screen public views, and located so as not to be the dominant feature along any street or within any development. The use of alternate materials to designate pedestrian areas within or adjacent to parking lots is encouraged, and pedestrian areas shall be separated from vehicular traffic with landscaping, decorative posts, special paving, or other measures to clearly define the pedestrian spaces. Property owners are encouraged to establish shared parking zones among uses on one or more lots.*
- *Materials. Minimum required masonry on all building elevations is 60%. Exterior walls not composed of masonry products shall not be covered with ribbed metal siding commonly referred to as corrugated metal.*
- *Design. Buildings and building features shall be sized and detailed appropriately for pedestrian use. Projected or recessed doorways and windows, awnings, and other architectural features may be used to achieve this design. Traditional strip-retail type frontages featuring long flat frontages with regularly spaced doors and unbroken expanses of concrete approaching the storefronts are specifically disallowed. Side and rear elevations shall be comprised of the same materials and reasonably similar in character and quality as the front elevation unless screened from view from all public streets and residential areas by topography differences, landscaping materials, or other screening devices, in which case building material may be concrete block or tilt-up concrete panels.*

It is staff's opinion that the site plan achieves consistence with the aforementioned design standards.

6. Additional Considerations:

The building is generally centered on the lot. Correspondence with the applicant indicated that this was to allow for potential expansion to its west. There is approximately 50' of green space to its east. The Taco Bell site has approximately 150' of green space to its west, so there is the potential for an additional development between the developments.



7. Staff Recommendation:

Findings:

1. The minor vehicle repair use of the property would be consistent with the current C-2-A District and the Comprehensive Plan Future Land Use Map designation of Commercial; and
2. The site plan would achieve consistency with North Liberty Code of Ordinances Section 165.05(2) entitled, "Preliminary Site Plan Review" and Section 169.10 entitled "Design Standards" and other Code of Ordinance requirements.

Recommendation:

Staff recommends the Planning Commission accept the two listed findings and forward the request to approve a site plan for a minor vehicle repair facility and related site improvements on approximately 1.5 acres of property located at 1040 Liberty Way to the City Council with a recommendation for approval.

Suggested motion:

I move that the Planning Commission accept the two listed findings and forward the site plan to the City Council with a recommendation for approval.

DRAWINGS FOR PROPOSED IMPROVEMENTS ARCHIE'S BRAKE AND QUICK LUBE 1040 LIBERTY WAY, NORTH LIBERTY, JOHNSON COUNTY, IOWA

LEGAL DESCRIPTION

LOT 2, LIBERTY CENTRE - PART ONE, NORTH LIBERTY, IOWA, ACCORDING TO THE PLAT THEREOF RECORDED IN BOOK 47, PAGE 107, PLAT RECORDS OF JOHNSON COUNTY, IOWA.

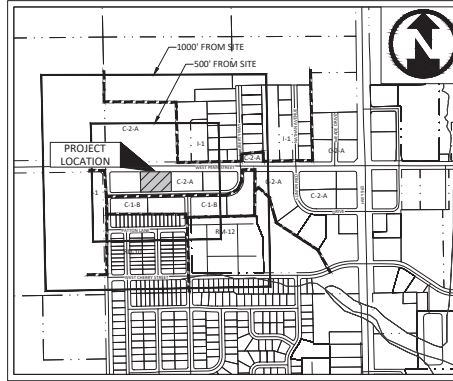
PROJECT SITE IS LOCATED IN SW 1/4 SECTION 12, TOWNSHIP 80 NORTH, RANGE 7 WEST. PARCEL ID IS 0612327003.

NOTE:

THE PROPOSED IMPROVEMENTS INCLUDED IN THESE DRAWINGS HAVE BEEN DESIGNED IN ACCORDANCE WITH CITY OF NORTH LIBERTY REQUIREMENTS AND THE IOWA STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS), LATEST ADDITION, UNLESS NOTED OTHERWISE ON THE PLANS.

NO DESIGN VARIANCES ARE REQUESTED.

STORM WATER MANAGEMENT FOR THIS SITE HAS BEEN ACCOUNTED FOR WITH THE ORIGINAL DEVELOPMENT OFFSITE.



CITY OF NORTH LIBERTY, IOWA

APPLICANT INFORMATION

OWNER/APPLICANT:

RON MACE
ARCHIES RENT A RIDE
760 NATHAN AVE #11
NORTH LIBERTY, IA 52317
319-800-5394

ATTORNEY:

DAVID BRIGHT
PUGH HAGAN PRAHAM PLC
CORALVILLE, IA 52241
319-351-2028

PROJECT INFORMATION

CONTACT PERSON:

BRIAN BOELK
AXIOM CONSULTANTS, LLC
60 E. COURT STREET, UNIT 3
IOWA CITY, IOWA 52240-3833
PHONE: 319-519-6220
EMAIL: BBOELK@AXIOM-CON.COM

UTILITY CONTACTS

ALLIANT ENERGY

CONTACT NAME: ALLIANT ENERGY FIELD ENGINEER
CONTACT PHONE: 800-255-4268
CONTACT EMAIL: LOCATE_IPL@ALLIANTENERGY.COM

LINN COUNTY RECORDER

CONTACT NAME: JOHNA NUNEMAKER
CONTACT PHONE: 319-377-1587
CONTACT EMAIL: NUNEMAKER@LINNCOUNTYREC.COM

MIDAMERICAN - GAS

CONTACT NAME: CARSON HEMPHILL
CONTACT PHONE: 319-341-4461
CONTACT EMAIL: CRHEMPHILL@MIDAMERICAN.COM

CITY OF NORTH LIBERTY

CONTACT NAME: TOM PALMER
CONTACT PHONE: 319-626-5736
CONTACT EMAIL: TPALMER@NORTHLIBERTY.ORG

SOUTH SLOPE TELEPHONE

CONTACT NAME: BRIAN FRESSE
CONTACT PHONE: 319-227-7111
CONTACT EMAIL: BRIAN@SOUTHSLOPE.COM

MEDIACOM IOWA CITY

CONTACT NAME: CARL NORTON
CONTACT PHONE: 319-594-6201
CONTACT EMAIL: CNORTON@MEDIACOMCC.COM

UNIVERSITY OF IOWA

CONTACT NAME: CHRIS HATLAND
CONTACT PHONE:
CONTACT EMAIL: CHRIS-HATLAND@UIOWA.EDU

WINDSTREAM COMMUNICATIONS

CONTACT NAME: LOCATE DESK
CONTACT PHONE: 800-289-1901
CONTACT EMAIL: LOCATE.DESK@WINDSTREAM.COM



SHEET INDEX	
C0.00	COVER SHEET
CIVIL	
C0.01	LEGEND AND GENERAL NOTES
C0.02	GENERAL NOTES
C1.00	TOPOGRAPHIC SURVEY AND REMOVALS PLAN
C2.00	OVERALL SITE AND UTILITY PLAN
C3.00	GRADING & EROSION CONTROL PLAN
C4.00	PAVEMENT PLAN
C5.00	CONSTRUCTION DETAILS
E1.00	PHOTOMETRIC PLAN
L1.00	LANDSCAPE PLAN

SITE INFORMATION

PURPOSE OF DEVELOPMENT

BRAKE AND LUBE SHOP

ZONING INFORMATION

CURRENT/PROPOSED ZONING C-1-B

MINIMUM LOT REQUIREMENTS

FRONT YARD SETBACK	25	FEET
SIDE YARD	10	FEET
REAR YARD	25	FEET

SITE CHARACTERISTICS

TOTAL LOT SIZE	65,174 SF	1.50 AC
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EXISTING:

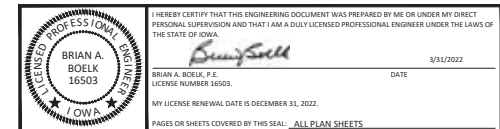
BUILDING AREA:	0 SF (0.00 AC)	0.0%
PAVEMENT AREA:	0 SF (0.00 AC)	0.0%
OPEN SPACE:	65,174 SF (1.50 AC)	100.0%
PRE-DEVELOPMENT IMPERVIOUS AREA:	0.0% (0.00 AC)	

DEVELOPED:

BUILDING AREA:	5,875 SF (0.14 AC)	9.3%
PAVEMENT AREA:	28,188 SF (0.65 AC)	43.3%
OPEN SPACE:	31,111 SF (0.71 AC)	47.4%
POST DEVELOPMENT IMPERVIOUS AREA:	52.6% (0.79 AC)	

PARKING

AUTOMOBILE REPAIR MINOR	2 STALLS PER SERVICE BAY (PROPOSED 5 SERVICE BAYS)
TOTAL REQUIRED PARKING	10 STALLS
TOTAL PROVIDED PARKING	43 STALLS (INCLUDES 2 ADA STALLS)



CITY APPROVAL

BY: _____

DATE: _____



ENGINEER

DRAWING LOG

REV	DATE	DESCRIPTION OF CHANGES
1	03-28-2022	CITY FIRST SUBMITTAL
2	03-23-2022	RESPONSE TO STAFF COMMENTS #1
3	03-31-2022	RESPONSE TO STAFF COMMENTS #2

ARCHIES BRAKE AND QUICK LUBE

NORTH LIBERTY, IA
SWITCH DEVELOPMENT

COVER SHEET

PROJECT NO: 220017
SHEET NO: BOELK

C0.00

LEGEND:

UTILITIES	EXISTING	PROPOSED
COMMUNICATIONS	— (CD) —	— CD —
OVERHEAD LINE	— (DH) —	— DH —
ELECTRIC	— (E) —	— E —
FIBER OPTIC	— (FD) —	— F —
GAS	— (G) —	— G —
SANITARY SEWER	— (S) —	— SS —
STORM	— (ST) —	— ST —
SUBURAIN	— (SU) —	— SD —
WATER: DOMESTIC	— (W) —	— W —
WATER: WELL	— (WELL) —	— OWELL —
CHILLED WATER: SUPPLY	— (CWD) —	— COWD —
CHILLED WATER: RETURN	— (CWRD) —	— COWRD —
CONDENSATE	— (CND) —	— CND —
STEAM: HIGH PRESSURE	— (HPS) —	— HPS —
STEAM: LOW PRESSURE	— (LPS) —	— LPS —
COMMUNICATIONS HANDHOLE	□	□
COMMUNICATIONS PEDESTAL	□	□
COMMUNICATIONS MANHOLE	□	□
GUY WIRE ANCHOR	•	•
UTILITY POLE	•	•
UTILITY POLE WITH LIGHT	•	•
LIGHT POLE	•	•
ELECTRIC MANHOLE	□	□
ELECTRIC TRANSFORMER	□	□
TRAFFIC HANDHOLE	□	□
TRAFFIC MANHOLE	□	□
FIBER OPTIC HANDHOLE	□	□
FIBER OPTIC MANHOLE	□	□
GAS MANHOLE	□	□
GAS VALVE	□	□
SANITARY SEWER MANHOLE	□	□
SANITARY SEWER CLEANOUT	□	□
STORM SEWER MANHOLE	□	□
STORM SEWER INTAKE	□	□
HYDRANT	□	□
WATER VALVE	□	□
CURB STOP	□	□
WATER MANHOLE	□	□
POST INDICATOR VALVE	□	□

SITE

	EXISTING	PROPOSED
CONTOUR - INDEX	— 100 —	— 100 —
CONTOUR - INTERMEDIATE	— 101 —	— 101 —
FENCE: BARB WIRE	— X — X — X —	— X — X — X —
FENCE: CHAIN LINK	— O — O — O —	— O — O — O —
FENCE: CONSTRUCTION	— — — — —	— — — — —
FENCE: VINYL	— — — — —	— — — — —
FENCE: WIRE	— — — — —	— — — — —
FENCE: WOOD	— — — — —	— — — — —
TREE LINE	— — — — —	— — — — —
DELINEATED WETLAND	— — — — —	— — — — —
100-YEAR FLOODPLAIN	— — — — —	— — — — —
500-YEAR FLOODPLAIN	— — — — —	— — — — —
STREAM CENTERLINE	— — — — —	— — — — —
BOLLARD	•	•
FLAG POLE	•	•
LANDSCAPE LIGHT	•	•
SHRUBBERY	•	•
TREE: DECIDUOUS	•	•
TREE: CONIFEROUS	•	•
MAIL BOX	•	•
POST	•	•
SIGN	•	•
PARKING METER	•	•
FIRE DEPARTMENT CONNECTION	•	•
WATER METER	•	•
HOSE BIB	•	•
YARD HYDRANT	•	•
DRINKING FOUNTAIN	•	•
MONITORING WELL	•	•
WELL	•	•

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS), 2022 EDITION, ALONG WITH THE NORTH LIBERTY SUPPLEMENT TO THE SUDAS (UPDATED OCTOBER 2021), UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- THE LOCATIONS OF UTILITY MAINS, STRUCTURES AND SERVICE CONNECTIONS PLOTTED ON THIS DRAWING ARE APPROXIMATE ONLY AND WERE OBTAINED FROM PLANS OF RECORD. THERE MAY BE OTHER EXISTING UTILITY MAINS, STRUCTURES AND SERVICE CONNECTIONS NOT KNOWN AND MAY NOT SHOWN ON THIS DRAWING.
- NOTIFY UTILITY COMPANIES WHOSE FACILITIES ARE SHOWN ON THE PLANS OR KNOWN TO BE WITHIN CONSTRUCTION LIMITS OF THE SCHEDULE PRIOR TO EACH STAGE OF CONSTRUCTION.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL UNCOVER EXISTING UTILITIES AT CRITICAL LOCATIONS TO VERIFY EXACT HORIZONTAL AND VERTICAL LOCATION.
- IOWA CODE 480, UNDERGROUND FACILITIES INFORMATION, REQUIRES VERBAL NOTICE TO IOWA ONE-CALL 1-800-292-8989, NOT LESS THAN 48 HOURS BEFORE EXCAVATING, EXCLUDING WEEKENDS AND HOLIDAYS.
- NOTIFY THE APPROPRIATE GOVERNING AUTHORITY 48 - 72 HOURS PRIOR TO BEGINNING CONSTRUCTION WITHIN PUBLIC RIGHT-OF-WAY. THE CITY OF NORTH LIBERTY SHALL BE THE PUBLIC AGENCY RESPONSIBLE FOR INSPECTION DURING CONSTRUCTION OF THE PUBLIC PORTIONS OF THE PROJECT.
- NO WORK SHALL BE PERFORMED BEYOND THE PROJECT LIMITS WITHOUT PRIOR AUTHORIZATION FROM THE OWNER OR OWNER'S REPRESENTATIVE.
- PROVIDE TRAFFIC AND PEDESTRIAN CONTROL MEASURES (SIGNS, BARRICADES, FLAGGERS, ETC.) IN COMPLIANCE WITH PART VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) LATEST EDITION.
- ADJUST ALL VALVES, MANHOLES, CASTINGS, GAS VENTS, ETC., TO MATCH THE NEW SURFACE. ADJUSTMENT SHALL BE COORDINATED WITH THE UTILITY COMPANIES AND THE COST FOR ALL ADJUSTMENTS SHALL BE INCIDENTAL TO THE CONSTRUCTION. AT NO ADDITIONAL COST TO THE OWNER, REPAIR ANY DAMAGE TO SAID STRUCTURES AND APPURTENANCES THAT OCCUR DURING CONSTRUCTION.
- REPLACE ANY PROPERTY MONUMENTS REMOVED OR DESTROYED BY CONSTRUCTION. MONUMENTS SHALL BE SET BY A LAND SURVEYOR REGISTERED TO PRACTICE IN THE STATE OF IOWA.

GRADING NOTES

- STRIP EXISTING VEGETATION WITHIN THE GRADING LIMITS AND AREAS TO RECEIVE FILL. STOCKPILE ON-SITE FOR REUSE IF SUITABLE.
- PROOF ROLL ALL FILL AREAS TO IDENTIFY SOFT OR DISTURBED AREAS IN THE SUBGRADE. PROOFROLL PER SUDAS SECTION 2010, 3.06, B.
- REMOVE AND RECOMPACT AREAS OF SUBGRADE WHICH ARE SOFT OR UNSTABLE TO MEET SPECIFIED LIMITS FOR DENSITY AND MOISTURE CONTENT.
- SCARIFY EXISTING SUBGRADE TO A DEPTH OF 12 INCHES AND RECOMPACT TO 98% OF STANDARD PROCTOR DENSITY (ASTM D698) PRIOR TO PLACEMENT OF FILL.
- DO NOT PLACE, SPREAD, OR COMPACT ANY FILL MATERIAL DURING UNFAVORABLE WEATHER CONDITIONS AND DO NOT RESUME COMPACTION OPERATIONS UNTIL MOISTURE CONTENT AND DENSITY OF IN-PLACE FILL MATERIAL ARE WITHIN SPECIFIED LIMITS.
- PLACE FILL MATERIAL IN 9" MAXIMUM LOOSE LIFTS.
- FILLS PLACED BELOW LAWN AREAS SHALL BE COMPACTED TO 90% OF MATERIALS MAXIMUM STANDARD PROCTOR DRY DENSITY (ASTM D698).
- SCARIFY SUBGRADE TO DEPTH OF 3 INCHES WHERE TOPSOIL IS SCHEDULED. SCARIFY AREAS WHERE EQUIPMENT USED FOR HAULING AND SPREADING TOPSOIL HAS CAUSED COMPACTED SUBSOIL.
- FILL MATERIAL OBTAINED FROM OFF-SITE SOURCES SHALL BE SOIL OR SOIL AND ROCK MIXTURE FREE FROM ORGANIC MATTER AND OTHER DELETERIOUS SUBSTANCES. IT SHALL CONTAIN NO ROCKS OF 6 INCHES IN GREATEST DIMENSION AND NOT MORE THAN 15% OF THE ROCKS SHALL BE LARGER THAN 2-1/2 INCHES IN GREATEST DIMENSION.
- SCARIFY AND RECOMPACT THE TOP 9" OF SUBGRADE IN ALL CUT AREAS AFTER ROUGH GRADING IS COMPLETED. COMPACT THE ENTIRE PAVING SUBGRADE TO 95% STANDARD PROCTOR DRY DENSITY TO WITHIN 1.0' OF FINAL SUBGRADE. THE FINAL 1.0' OF FILL TO BE COMPACTED TO 98% STANDARD PROCTOR DRY DENSITY (ASTM D698).
- IN AREAS TO RECEIVE ADDITIONAL FILL OVER EXISTING FILL MATERIALS. REMOVE TOP 12" OF MATERIAL AND SCARIFY AND RECOMPACT THE NEXT 9" OF RESULTING SUBGRADE. COMPACT RESULTING SUBGRADE TO 95% STANDARD PROCTOR DRY DENSITY. SUBSEQUENT FILL TO BE COMPACTED TO 98% STANDARD PROCTOR DRY DENSITY TO WITHIN 1.0' OF FINAL SUBGRADE. THE FINAL 1.0' OF FILL TO BE COMPACTED TO 98% STANDARD PROCTOR DRY DENSITY (ASTM D698).
- FINISH CONTOURS SHOWN ARE TO TOP OF FINISHED GRADE OR TO TOP OF TOPSOIL.

SITE PREPARATION NOTES

- PROTECT ADJACENT PROPERTY DURING DEMOLITION.
- DEMOLITION LIMIT LINE IS THE EXISTING PROPERTY LINE UNLESS NOTED OTHERWISE.
- MAINTAIN POSITIVE DRAINAGE ON THE SITE THROUGHOUT THE PROJECT DURATION.
- PROVIDE WASTE AREAS OR DISPOSAL SITES FOR EXCESS MATERIAL (EXCAVATED MATERIAL OR BROKEN CONCRETE) WHICH IS NOT DESIRABLE TO BE INCORPORATED INTO THE WORK INVOLVED ON THIS PROJECT. NO PAYMENT FOR OVERHAUL WILL BE ALLOWED FOR MATERIAL HAULED TO THESE SITES. NO MATERIAL SHALL BE PLACED WITHIN THE EASEMENTS, UNLESS SPECIFICALLY STATED IN THE PLANS OR APPROVED BY THE ENGINEER. DISPOSAL SITES MUST BE APPROVED BY THE ENGINEER. CONTRACTOR SHALL APPLY NECESSARY MOISTURE TO THE CONSTRUCTION AREA AND TEMPORARY HAUL ROADS TO PREVENT THE SPREAD OF DUST. OFF-SITE DISPOSAL SHALL BE IN ACCORDANCE WITH THE APPLICABLE GOVERNMENTAL REGULATIONS.
- KEEP ADJACENT PUBLIC STREETS FREE FROM SOIL AND DEBRIS GENERATED BY THE PROJECT. CLEAN SOIL AND DEBRIS FROM THE ADJACENT STREETS ON A DAILY BASIS.
- DURING CONSTRUCTION, CONTROL DUST SPREADING FROM ALL WORK AND STAGING AREAS.
- REMOVAL OR ABANDONMENT OF PUBLIC UTILITIES SHALL BE FULLY COORDINATED WITH APPROPRIATE UTILITY SUPPLIER AND REGULATORY AGENCIES.
- ANY EXISTING FACILITIES (CURBS, PAVEMENT, UTILITIES, ETC.) THAT THE CONTRACTOR'S OPERATIONS DAMAGE SHALL BE REPAIRED BY THAT CONTRACTOR AT HIS/HER COST.
- REMOVE ALL DESIGNATED STREETS, DRIVEWAYS, ETC. IN THEIR ENTIRETY. BACKFILL ALL EXCAVATIONS WITH COHESIVE MATERIAL COMPACTED TO 98% STANDARD PROCTOR DRY DENSITY (ASTM D698).
- WHERE A SECTION OF PAVEMENT, CURB AND GUTTER OR SIDEWALK IS CUT OR OTHERWISE DAMAGED BY THE CONTRACTOR, THE ENTIRE SECTION SHALL BE REMOVED AND REPLACED. PAVEMENT, CURBS, GUTTERS AND SIDEWALKS SHALL BE REMOVED A MINIMUM OF TWO FEET BEYOND THE EDGE OF THE TRENCH CUT AND TO THE NEAREST JOINT.
- SAWCUT EDGES OF PAVEMENT FULL DEPTH PRIOR TO REMOVAL TO PREVENT DAMAGE TO ADJACENT SLABS AND FIXTURES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL EXISTING CONCRETE STRUCTURES ON THE SITE AS SHOWN ON THE PLANS. THE REMOVAL INCLUDES DRIVEWAYS, CURB AND GUTTER, SIDEWALK, AND BASEMENT FOUNDATION FOOTINGS, FLOOR AND WALLS. THE REMOVAL ALSO INCLUDES STORM SEWER INTAKES AND PIPE AS SHOWN ON THE PLANS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT TREES AND SHRUBS NOTED ON THE PLANS TO REMAIN IN PROJECT AREA FROM DAMAGE DUE TO CONSTRUCTION ACTIVITY. PROTECTION INCLUDES, BUT IS NOT LIMITED TO, CONSTRUCTION FENCING AROUND THE DRIP LINE OF TREES AND PROHIBITING VEHICLE TRAFFIC WITHIN THE DRIP LINE OF TREES.
- REMOVAL AND DISPOSAL OF EXISTING TREES AND SHRUBS WITHIN CONSTRUCTION LIMITS SHALL BE INCIDENTAL TO THE GRADING PORTION OF THE PROJECT. STUMPS ARE TO BE GROUND TO TWO FEET BELOW FINISHED GRADE.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF A TILE IS ENCOUNTERED AND SHALL INDICATE THE METHOD OF RESOLVING THE CONFLICT. THE ENGINEER SHALL APPROVE THE PROPOSED METHOD. THE LOCATION OF THE FIELD TILE SHALL BE RECORDED ON THE CONSTRUCTION RECORD DOCUMENTS.

EXISTING FIELD TILE LINES ENCOUNTERED IN THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR IN ONE OF THE FOLLOWING WAYS:

- CONNECT TILE TO THE NEAREST STORM SEWER.
- DAYLIGHT TO FINISHED GROUND.
- REPAIR TILE AND MAINTAIN SERVICE.

SURFACE RESTORATION NOTES

- ONLY DISTURBED AREAS NOT PAVED OR HARD SURFACED, ADJACENT TO THE PROPOSED BUILDING AND PAVING, SHALL RECEIVE MINIMUM 4" TOPSOIL. SCARIFY AREAS TO RECEIVE TOPSOIL TO A MIN. DEPTH OF 4". REMOVE ALL STONES, WOOD AND DEBRIS LARGER THAN 2" FROM AREAS TO RECEIVE TOPSOIL. DO NOT COMPACT TOPSOIL.
- ALL DISTURBED AREAS SHALL BE SEEDED, FERTILIZED AND MULCHED IN ACCORDANCE WITH SUDAS SECTION 9010.
- SEED ALL DISTURBED AREAS NOT TO BE HARD SURFACED, AND NOT TO HAVE TOPSOIL SPREAD, WITH TYPE 1 EROSION CONTROL MIXTURE PER SUDAS SECTION 9010.
- APPLY SEED AT THE RATES INDICATED IN THE PROJECT SPECIFICATIONS.
- MAINTAIN SEEDED AREAS UNTIL AN ADEQUATE STAND OF GRASS HAS BEEN ESTABLISHED. RESEED ANY AREAS AS NECESSARY TO STABILIZE SOIL PER PROJECT SPECIFICATIONS.
- EXISTING FACILITIES (CURBS, PAVEMENT, UTILITIES, ETC.) THAT ARE TO REMAIN AND DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL EXPENSE TO THE OWNER.

EROSION CONTROL NOTES

- THE CONTRACTOR SHALL PROVIDE TEMPORARY EROSION CONTROL, SEDIMENT, AND DUST CONTROL IN ACCORDANCE WITH THE REQUIREMENTS OF THE PROJECT'S STORM WATER POLLUTION PREVENTION PLAN AND THE STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS), UNLESS OTHERWISE NOTED. DISTURBED AREA WILL BE 3.90 ACRES. AN NPDES GENERAL PERMIT #2 IS REQUIRED PRIOR TO LAND DISTURBING ACTIVITIES.
- THE CONTRACTOR SHALL INCORPORATE ALL EROSION CONTROL FEATURES INTO THE PROJECT PRIOR TO DISTURBING THE SOIL.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO INSPECT THE TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES A MINIMUM OF ONCE PER WEEK. IF A CONTROL MEASURE HAS BEEN REDUCED IN CAPACITY BY 50% OR MORE, THE CONTRACTOR SHALL RESTORE SUCH FEATURES TO THEIR ORIGINAL CONDITION IMMEDIATELY, WEATHER PERMITTING.
- ERECT SILT FENCE AS SHOWN ON THE PLANS TO LIMIT LOSS OF MATERIAL FROM THE SITE. DEVICES TO REMAIN IN PLACE AND TO BE MAINTAINED UNTIL A PERMANENT GROUND COVER IS ESTABLISHED.
- MINIMIZE SOIL EROSION BY MAINTAINING ALL EXISTING VEGETATIVE GROWTH WITHIN THE GRADING LIMITS FOR AS LONG AS PRACTICAL.
- INSTALL A SILT FENCE AROUND ALL STOCKPILED TOPSOIL.
- THE CONTRACTOR SHALL PROVIDE TEMPORARY SEEDING FOR ALL AREAS THAT ARE DISTURBED AND OPERATIONS WILL NOT COMMENCE OR PERMANENT SEEDING WILL NOT BE COMPLETED IN LESS THAN 14 DAYS.
- SEQUENCE OF EROSION AND SEDIMENT CONTROL EVENTS:
 - INSTALL INLET PROTECTION AROUND EXISTING INTAKES AS INDICATED ON THE SITE CONSTRUCTION PLAN. USE THESE LOW AREAS AS SEDIMENT BASINS DURING CONSTRUCTION.
 - INSTALL PERIMETER SILT FENCE AS INDICATED ON THE SITE CONSTRUCTION PLAN.
 - INSTALL SILT FENCE AROUND ANY TOPSOIL OR EXCESS SOIL STOCKPILES. APPLY TEMPORARY SEEDING TO ALL TOPSOIL OR EXCESS SOIL STOCKPILES.
 - INSTALL STONE SUBBASE ON STREET AREAS FOLLOWING COMPLETION OF GRADING.
 - APPLY TEMPORARY SEEDING TO ALL DENUDED AREAS WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR 14 DAYS OR MORE. FERTILIZE AND LIME IF NEEDED. APPLY MULCH ON SLOPES GREATER THAN 4:1 (HORIZONTAL:VERTICAL).
 - DESTROY TEMPORARY SEEDING AND APPLY PERMANENT SEEDING TO ALL DISTURBED AREAS NOT TO BE HARD SURFACED. FERTILIZE AND MULCH PERMANENT SEEDING AS REQUIRED. APPLY MULCH AT 1.5 TO 2.0 TONS PER ACRE ON SLOPES GREATER THAN 4:1.
 - WHEN CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED WITH PERMANENT SEEDING, REMOVE ACCUMULATED SEDIMENT FROM ANY SEDIMENT BASINS, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVALS.

STORMWATER POLLUTION PREVENTION:

THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) SHALL BE KEPT ON THE CONSTRUCTION SITE AT ALL TIMES FROM THE DATE CONSTRUCTION ACTIVITIES BEGIN TO THE DATE OF FINAL STABILIZATION. THE CONTRACTOR SHALL MAINTAIN THE SWPPP PER THE REQUIREMENTS OF GENERAL PERMIT NO. 2. ALL OPERATORS/CONTRACTORS WORKING ON-SITE MUST SIGN THE CERTIFICATION STATEMENT PROVIDED AND WILL BECOME CO-PERMITTEES ON THE NPDES GENERAL PERMIT NO. 2 FOR THIS SITE. ALL OPERATORS/SUBCONTRACTORS WORKING ON-SITE SHALL BE SUPPLIED A COPY OF THE SWPPP BY THE CONTRACTOR AND MUST BE FAMILIAR WITH ITS CONTENTS. THE SWPPP MUST BE PERIODICALLY UPDATED TO SHOW CURRENT EROSION CONTROL PRACTICES PER THE REQUIREMENTS OF THE GENERAL PERMIT #2. UPDATED VERSIONS OF THE SWPPP WILL BE PROVIDED TO ALL OF THE OPERATORS/SUBCONTRACTORS WHOSE AREAS ARE AFFECTED BY THE CHANGES MADE TO THE SWPPP. IT WILL BE THE DUTY OF THE CONTRACTOR TO SEE THAT THESE REQUIREMENTS ARE MET.

STORM SEWER NOTES

1. RCP STORM SEWER SHALL BE CLASS III REINFORCED CONCRETE PIPE (RCP) TO DEPTH OF COVER UP TO 12' CONFORMING TO ASTM C76 OR AASHTO M170.
2. RCP STORM SEWER SHALL BE CONSTRUCTED WITH CLASS R-2 BEDDING PER SUDAS FIGURE 3010.102 (DOT STANDARD ROAD PLAN SW-102). STORM SEWERS MAY BE BACKFILLED WITH SUITABLE NATIVE MATERIAL.
3. HIGH DENSITY POLYETHYLENE PIPE (HDPE) SHALL BE CORRUGATED WITH INTEGRALLY FORMED SMOOTH INTERIOR MEETING THE REQUIREMENTS OF THE STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS) SECTION 4020.
4. HDPE STORM SEWER SHALL BE CONSTRUCTED WITH CLASS F-3 BEDDING. HDPE STORM SEWER SHALL BE BACKFILLED WITH GRANULAR BACKFILL AGGREGATE TO A MINIMUM OF 12" ABOVE THE TOP OF THE PIPE.
5. STORM SEWER LINES SHALL BE A MINIMUM OF 10' FROM WATER LINES RUNNING PARALLEL. AT CROSSINGS, A MINIMUM 18" SEPARATION MUST BE PROVIDED.
6. ALL LINE AND GRADE CONTROL WILL BE DONE WITH A LASER BEAM, WITH GRADE CHECKS AT 25', 50' AND THEN EVERY 100' BETWEEN MANHOLES.
7. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN A RECORD DRAWING SET SHOWING LOCATIONS OF ALL STORM SEWER CONSTRUCTION. THE RECORD DRAWING SET WILL BE PROVIDED TO THE OWNER.

PCC PAVEMENT NOTES

1. PCC PAVING THICKNESS SHALL BE 6-INCH ON 6-INCH GRANULAR SUBBASE, UNLESS STATED OTHERWISE.
2. MATERIALS AND CONSTRUCTION FOR PORTLAND CEMENT CONCRETE PAVEMENTS SHALL MEET THE REQUIREMENT OF IOWA DOT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, LATEST REVISION, SECTION 2301. THE PARAGRAPHS FOR MEASUREMENT AND PAYMENT SHALL NOT APPLY.
3. MINIMUM 28-DAY COMPRESSIVE STRENGTH FOR CONCRETE USED FOR PAVEMENTS SHALL BE 4000 PSI. CONCRETE SHALL BE C-3 OR C-4 WITH TYPE 1 CEMENT. AIR CONTENT SHALL BE 6-1/2% ± 1.5% COARSE AGGREGATE. AIR ENTRAINMENT ADMIXTURES AND WATER REDUCING ADMIXTURES SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4103. DURABILITY FOR PORTLAND CEMENT CONCRETE SHALL BE CLASS 2. JOINT SEALER SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4136 FOR HOT POURED JOINT SEALER.
4. JOINT SEALER SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4136 FOR HOT POURED JOINT SEALER.
5. CURING COMPOUND (WHITE, DARK OR CLEAR) SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR SECTION 4105. APPLICATION METHOD AND CURING SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 2301.19.
6. FLYASH PER IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4108 MAY BE SUBSTITUTED FOR CEMENT AT THE RATES SPECIFIED IN SECTION 2301.04E AFTER NOTIFICATION AND AUTHORIZATION BY THE OWNER'S REPRESENTATIVE.
7. PAVEMENT TIE BARS AND DOWEL BARS SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4151. EPOXY COATING, WHEN SPECIFIED, SHALL CONFORM TO IOWA DOT SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4151.03B.
8. CURBS SHALL BE CAST INTEGRAL WITH CONCRETE PAVEMENT UNLESS NOTED OTHERWISE.
9. PCC SIDEWALKS 5 FEET OR LESS IN WIDTH SHALL BE 4 INCHES THICK. PCC SIDEWALKS GREATER THAN 5 FEET WIDE BUT LESS THAN 12 FEET WIDE SHALL BE 6 INCHES THICK. SIDEWALK THICKNESS AT ALL DRIVEWAYS SHALL MATCH THAT OF THE ADJACENT DRIVEWAY. TRANSVERSE CONSTRUCTION JOINT SPACING FOR PCC SIDEWALKS 5 FEET OR LESS IN WIDTH SHALL BE PLACED A MAXIMUM OF 5 FEET ON CENTER. TRANSVERSE CONSTRUCTION JOINT SPACING FOR PCC SIDEWALKS GREATER THAN 5 FEET WIDE BUT LESS THAN 12 FEET WIDE SHALL BE PLACED A MAXIMUM OF 8 FEET ON CENTER. PLACE EXPANSION JOINTS WHERE WALK MEETS OTHER WALKS, BACK OF CURBS, FIXTURES, OR OTHER STRUCTURES, AND AT INTERVALS NOT EXCEEDING 50 FEET. SIDEWALKS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2% DRAINING TOWARDS BACK OF CURB, UNLESS OTHERWISE NOTED.
10. ONE INCH PREFORMED FOAM EXPANSION JOINT MATERIAL SONOFLEX "F" BY SONOBORN OR APPROVED EQUAL SHALL BE PLACED BETWEEN NEW PAVEMENT CONSTRUCTION AND THE FACES OF BUILDINGS, STOOPS, EXISTING SLABS, AND OTHER FIXTURES, UNLESS NOTED ON THE DRAWINGS. JOINTS AT THESE LOCATIONS SHALL BE SEALED WITH A SELF-LEVELING POLYURETHANE SUCH AS SONOLASTIC SL-1 OR APPROVED EQUAL.
11. CONSTRUCT 1" EXPANSION JOINTS ON PCC CURB AT ALL ENDS OF RETURN RADII.

PAVEMENT GENERAL NOTES

1. ALL SLOPES IN PAVEMENT SHALL BE UNIFORM TO AVOID PONDING.
2. ALL DIMENSIONS TO BACK-OF-CURB UNLESS NOTED OTHERWISE.
3. REMOVE AND REPLACE OR RESTORE ALL STREET SIGNS, PAVEMENT MARKINGS, SIDEWALK LAMPS, SIDEWALKS, STEPS, LANDSCAPE STRUCTURES, CURB AND GUTTER, STREETS, DRIVES AND ALL OTHER SURFACE STRUCTURES REMOVED OR OTHERWISE DAMAGED DURING THE COURSE OF THE WORK. SIDEWALKS SHALL BE REMOVED AND REPLACED TO NEAREST JOINT BEYOND CONSTRUCTION AREA, UNLESS NOTED OTHERWISE. STREET AND SIDEWALK PANELS SHALL BE REMOVED IN FULL PANELS ONLY.
4. COMPACT SUBGRADE BENEATH PAVEMENTS IN ACCORDANCE WITH GRADING NOTES.
5. GRANULAR SUBBASE FOR PAVEMENTS SHALL MEET THE LIMITS OF GRADUATION NO. 14 (MODIFIED SUBBASE) PER IOWA DOT STANDARD SPECIFICATION FOR HIGHWAY AND BRIDGE CONSTRUCTION SECTION 4123.
6. PROOF-ROLL SUBGRADE PER GRADING NOTES.
7. ALL CURB AND GUTTER IS 6" STANDARD CURB UNLESS STATED OTHERWISE.
8. PAVING AT DAYS WORK JOINT: PROVIDE #4 EPOXY COATED REBAR, 18" LONG, 24" ON CENTER SPACING. DRILL IN 6". ANCHOR WITH 2-PART EPOXY.
9. PAVEMENT IN FIRE LANE CAN SUPPORT FIRE APPARATUS WEIGHING 75,000 LBS.

WATER MAIN NOTES (SUDAS):

1. WATER LINES SHALL BE CONSTRUCTED PER SUDAS SPECIFICATIONS, 2022 EDITION, ALONG WITH THE NORTH LIBERTY SUPPLEMENT TO THE SUDAS (UPDATED OCTOBER 2021), AND LIST OF ACCEPTED PRODUCTS FOR WATER DISTRIBUTION MATERIALS.
2. WATER LINES SHALL BE CONSTRUCTED OF THE FOLLOWING MATERIALS PER SUDAS SECTION 5010:

PIPE MATERIAL	BEDDING CLASS
PVC AWWA C900	P-1
JOINTS AND FITTING	BEDDING CLASS
MECHANICALLY RESTRAINED DUCTILE IRON	P-1
3. ALL WATER MAINS AND SERVICE LINES SHALL BE INSTALLED WITH TRACER WIRE
4. WATER MAIN FITTINGS SHALL BE MECHANICAL JOINTS. MEGALUG 2000PV FOR PVC PIPE.
5. CONCRETE THRUST BLOCKS COMPLYING WITH SUDAS FIGURE 5010.101 SHALL BE USED FOR PIPE SIZES SIXTEEN INCHES AND SMALLER AND ARE REQUIRED AT ALL HYDRANTS, BENDS, VALVES, CROSSES, AND TEES.
6. CURB STOPS SHALL BE LOCATED AS INDICATED ON THE PLANS. MAINTAIN 36" HORIZONTAL SEPARATION BETWEEN SEWER AND WATER SERVICES. THE ENDS OF WATER SERVICES SHALL BE MARKED WITH A 2x4 POSTS PAINTED BLUE.
7. CONTRACTOR MAY DEFLECT WATER MAIN PER MANUFACTURER'S SPECIFICATIONS FOR HORIZONTAL DEFLECTION. PIPE DEFLECTION IS NOT ALLOWED FOR VERTICAL DEFLECTIONS.
8. THE APPROVING AUTHORITY SHALL OPERATE ALL EXISTING SYSTEM VALVES AND THE CONTRACTOR AND CITY SHALL COORDINATE ANY SHUT DOWNS TO THE EXISTING SYSTEM. THE CONTRACTOR SHALL MAKE ACCOMMODATIONS IN THE SCHEDULE TO ALLOW FOR ADEQUATE PUBLIC NOTICE AS REQUIRED BY THE APPROVING AUTHORITY.
9. CONSTRUCTION OF PUBLIC AND PRIVATE WATER SYSTEM IMPROVEMENTS REQUIRES AN APPROVED PLAN SET ON FILE WITH THE APPROVING AUTHORITY. THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE APPROVING AUTHORITY FOR ANY VARIANCE FROM THE APPROVED PLAN.
10. ALL TAPS FOUR INCHES AND LARGER SHALL BE "LIVE" TAPS USING AN APPROVED TAPPING SLEEVE AND VALVE, UNLESS NOTED OTHERWISE. THE APPROVING AUTHORITY SHALL WITNESS AND/OR PERFORM ALL TAPS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING AND ARRANGING ON SITE INSPECTION OF WATER MAIN INSTALLATION WITH THE APPROVING AUTHORITY.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TESTING AND RELATED COSTS INCLUDING, BUT NOT LIMITED TO, FILLING AND FLUSHING NEW MAINS, AND ALL REQUIRED SAMPLING. DISPOSAL OF HIGHLY CHLORINATED WATER SHALL BE IN COMPLIANCE WITH STATE, LOCAL, AND FEDERAL REGULATIONS. ALL TESTING TO COMPLY WITH THE REQUIREMENTS OF SUDAS SECTION 5030. ALL INSPECTION AND TESTING COSTS ARE INCIDENTAL TO THE PROJECT AT NO ADDITIONAL COST TO THE OWNER.
13. THE MINIMUM DEPTH OF BURY FOR ALL WATER MAINS AND WATER SERVICES IS FIVE FEET SIX INCHES (5' - 6"), MEASURED FROM FINAL GRADE TO TOP OF WATER MAIN PIPE, UNLESS NOTED OTHERWISE.

DESIGN STANDARDS AND REFERENCE DRAWINGS

THE FOLLOWING SUDAS FIGURES ARE INCLUDED BY REFERENCE:	
FIGURE	TITLE
3010.101	TRENCH BEDDING AND BACKFILL ZONES
3010.102	RIGID GRAVITY PIPE TRENCH BEDDING
3010.103	FLEXIBLE GRAVITY PIPE TRENCH BEDDING
4040.231	SUBDRAINS
4040.232	SUBDRAIN CLEANOUTS
4040.233	SUBDRAIN OUTLETS
6010.401	CIRCULAR STORM SEWER MANHOLE
6010.602	CASTINGS FOR STORM SEWER MANHOLES
7010.101	JOINTS
7010.102	PCC CURB DETAILS
7010.103	MANHOLE BOXOUTS IN PCC PAVEMENT
7030.101	CONCRETE DRIVEWAY, TYPE A
7030.202	CURB DETAILS FOR CLASS A SIDEWALK
7030.206	CURB RAMPS OUTSIDE OF INTERSECTION RADIUS
7030.210	DETECTABLE WARNING PLACEMENT
7040.101	FULL DEPTH PCC PATCHES LESS THAN OR EQUAL TO 15' LONG
8030.101	TEMPORARY TRAFFIC CONTROL GENERAL INFORMATION
8030.102	WORK OFF PAVEMENT WITH MINOR ENCROACHMENT ONTO TRAVELED WAY
8030.117	SIDEWALK DETOUR
9030.101	PLANTING PIT
9030.102	TREE STAKING, GUYING, AND WRAPPING
9040.102	FILTER BERM AND FILTER SOCK
9040.119	SILT FENCE
9040.120	STABILIZED CONSTRUCTION ENTRANCE

PROJECT NAME:
ARCHES BRAKE AND QUICK LUBE

CLIENT NAME:
NORTH LIBERTY, IA

PROJECT NO. / SHEET NUMBER:
220017 / BOELK

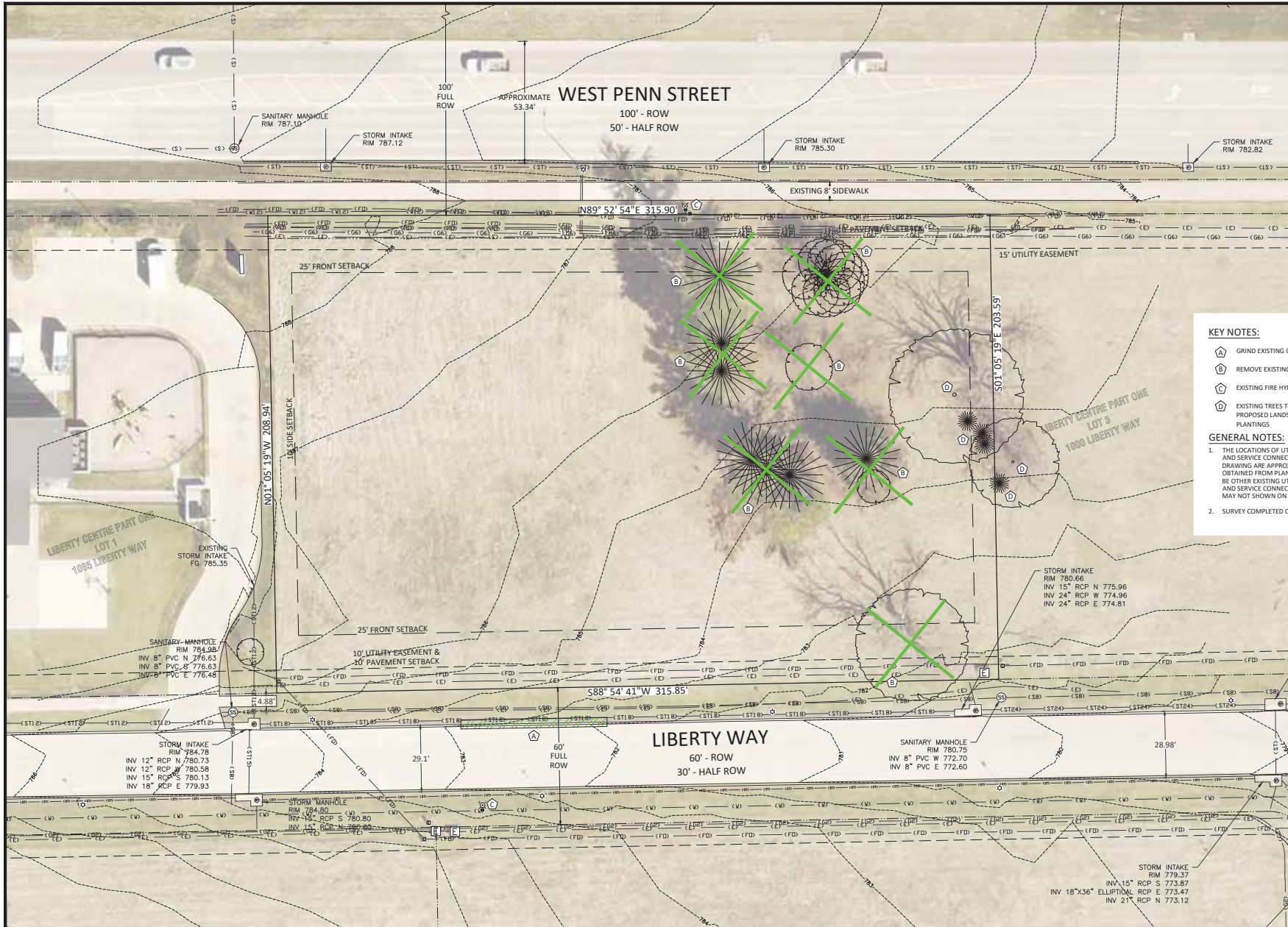
DRAWING LOG

REV.	DATE	DESCRIPTION OF CHANGES
1	03-28-2022	CITY FIRST SUBMITTAL
2	03-23-2022	RESPONSE TO STAFF COMMENTS #1
3	03-31-2022	RESPONSE TO STAFF COMMENTS #2

ENGINEER



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- KEY NOTES:**
- (A) GRIND EXISTING CURB
 - (B) REMOVE EXISTING TREE
 - (C) EXISTING FIRE HYDRANT ASSEMBLY
 - (D) EXISTING TREES TO REMAIN. SEE L.L.00 FOR PROPOSED LANDSCAPING LIMITS AND PLANTINGS
- GENERAL NOTES:**
- THE LOCATIONS OF UTILITY MAINS, STRUCTURES AND SERVICE CONNECTIONS PLOTTED ON THIS DRAWING ARE APPROXIMATE ONLY AND WERE OBTAINED FROM PLANS OF RECORD. THERE MAY BE OTHER EXISTING UTILITY MAINS, STRUCTURES AND SERVICE CONNECTIONS NOT KNOWN AND MAY NOT SHOWN ON THIS DRAWING.
 - SURVEY COMPLETED ON FEBRUARY 04, 2022.

NOT FOR CONSTRUCTION

TOPOGRAPHIC SURVEY AND REMOVALS PLAN

220017 BOELK

C1.00

ARCHES BRAKE AND QUICK LUBE

NORTH LIBERTY, IA

SWITCH DEVELOPMENT

PROJECT NAME: ARCHES BRAKE AND QUICK LUBE

CLIENT NAME: NORTH LIBERTY, IA

PROJECT NO.: 220017

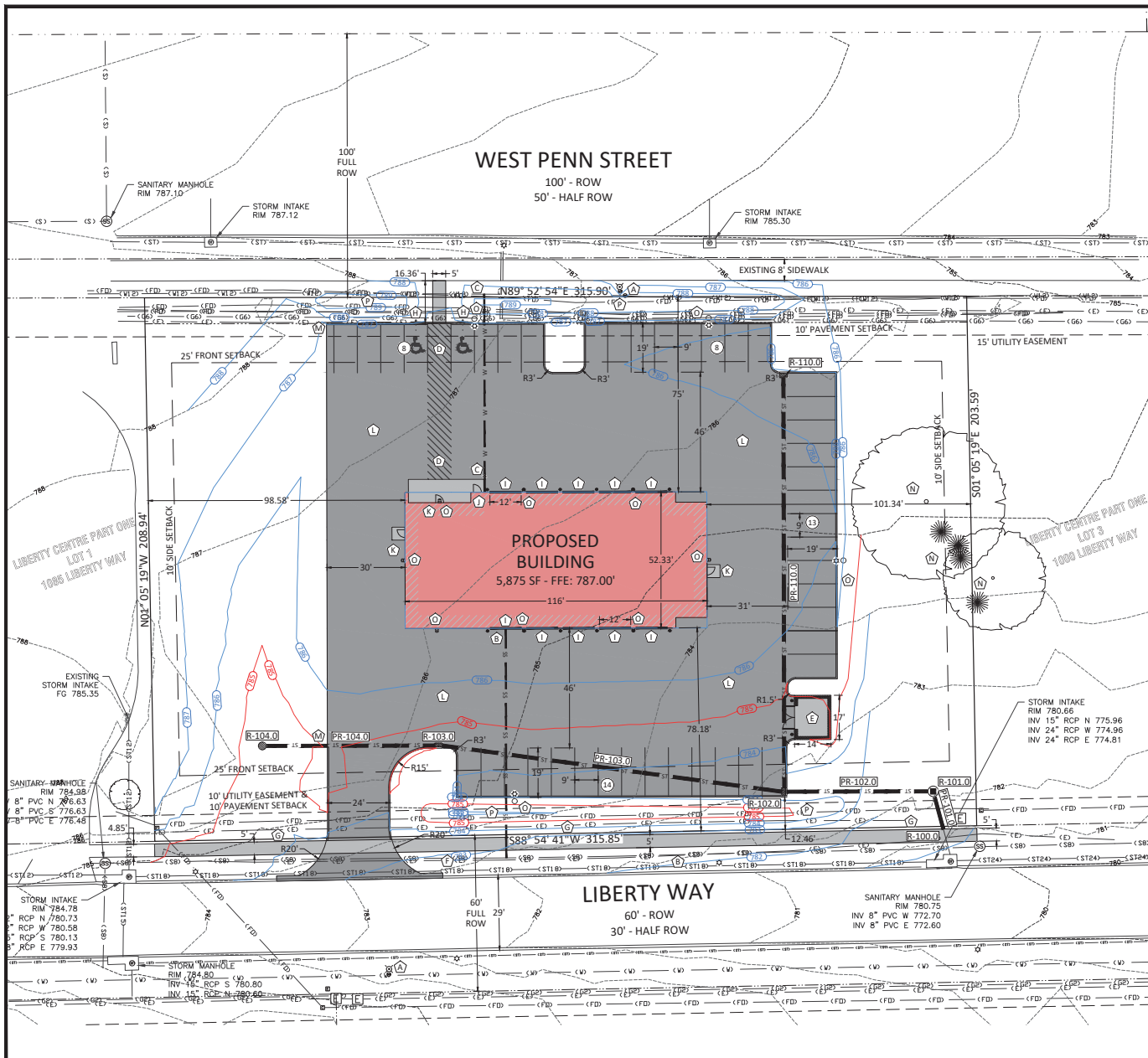
DESIGNER/PERSONAL: BOELK

DRAWING LOG

REV	DESCRIPTION OF CHANGES	DATE
1	CITY FIRST SUBMITTAL	02-28-2022
2	RESPONSE TO DATE COMMENTS #1	03-23-2022
3	RESPONSE TO DATE COMMENTS #2	03-31-2022

ENGINEER:

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SITE & UTILITY KEYNOTES:

- EXISTING FIRE HYDRANT
- PROPOSED SANITARY SEWER SERVICE. COORDINATE WITH MEP PLANS FOR FINAL SIZE OF SERVICE. OIL SEPARATOR TO BE LOCATED WITHIN BUILDING.
- LIVE TAP EXISTING WATERMAIN AND PROVIDE ±76 LF OF 6" PROPOSED WATER SERVICE TO BUILDING. CONTRACTOR TO PROVIDE CURB STOP 5' FROM FACE OF BUILDING.
- PROPOSED GORE AREA PAVEMENT MARKING FROM PEDESTRIAN ACCESS.
- PROPOSED DUMPSTER ENCLOSURE. SEE DETAIL C5.00
- PROPOSED SUDAS CONCRETE DRIVEWAY, TYPE A. SUDAS 7030.101
- PROPOSED SUDAS CLASS B SIDEWALK. SUDAS 7030.201. SEE DETAIL C5.00
- PROPOSED ADA PARKING SIGN
- PROPOSED GARAGE DOOR WITH PROPOSED BOLLARDS. SEE DETAIL C5.00.
- PROPOSED SPRINKLER ROOM DOOR ACCESS & STOOP.
- PROPOSED EXTERIOR DOOR ACCESS & STOOP.
- PROPOSED STANDARD DUTY PAVEMENT. SEE DETAIL C5.00
- END STANDARD CURB SECTION. WEST EDGE OF PAVEMENT TO HAVE NO CURB.
- PROTECT EXISTING TREES DURING CONSTRUCTION ACTIVITIES.
- PROPOSED LIGHTING FIXTURE. SEE LIGHTING SITE PLAN E1.00.
- PROPOSED LANDSCAPING BERM. SEE SECTION A-A AND DETAIL A ON SHEET C3.00.

STORM SEWER STRUCTURES

STRUCTURE ID	TYPE	RIM / FG	INFLOW PIPE IE	OUTFLOW PIPE IE
100.0	CONNECT TO EXISTING STUB	781.56	15" IE: 776.06 N	
101.0	48" SW-402	782.06	15" IE: 777.49 W	15" IE: 776.21 S
102.0	SW-501	783.34	15" IE: 778.73 W	15" IE: 778.63 E
103.0	SW-501	784.48	12" IE: 780.32 W	15" IE: 780.07 E
104.0	18" SW-512	784.07		12" IE: 781.00 E
110.0	SW-501	785.37		15" IE: 781.93 S

STORM SEWER PIPE TABLE

PIPE ID	STRUCTURE US	STRUCTURE DS	INVERT US	INVERT DS	LENGTH (FEET)	SLOPE	SIZE (IN)	NOTES
101.0	101.0	100.0	776.21	776.06	15.1	1.00%	15	RCP
102.0	102.0	101.0	778.63	777.49	57.3	2.00%	15	RCP
103.0	103.0	102.0	780.07	778.73	134.0	1.00%	15	RCP
104.0	104.0	103.0	781.00	780.32	67.8	1.00%	12	HDPE
110.0	110.0	102.0	781.93	779.51	160.9	1.50%	15	RCP

GENERAL NOTES:

- PVC WATER SERVICE TO HAVE TRACER WIRE IN ACCORDANCE WITH SUDAS AND NORTH LIBERTY WATER DEPARTMENT REQUIREMENTS.

LEGEND

- SIDEWALK PCC PAVEMENT
- STANDARD DUTY PCC PAVEMENT



A SIDEWALK AT ADA STALLS TO SLOPE DOWN TO GUTTER
ELEVATION AT 12:1 SLOPE (MAX)

B BEGIN TRANSITION FROM NO CURB TO STANDARD CURB. SEE
PCC CURB DETAILS SUDAS PV-102

1. JOINTING PLAN TO BE SUBMITTED BY PAVING CONTRACTOR FOR ENGINEER REVIEW PRIOR TO PLACEMENT.
2. SEE SUDAS SECTION 7010 FOR ADDITIONAL CONCRETE PAVING REQUIREMENTS

TW = TOP OF WALK
TC = TOP OF CURB
TP = TOP OF PAVEMENT
GT = GUTTER
3. ASSUME STANDARD 6" CURB UNLESS NOTED OTHERWISE.

NOT FOR CONSTRUCTION

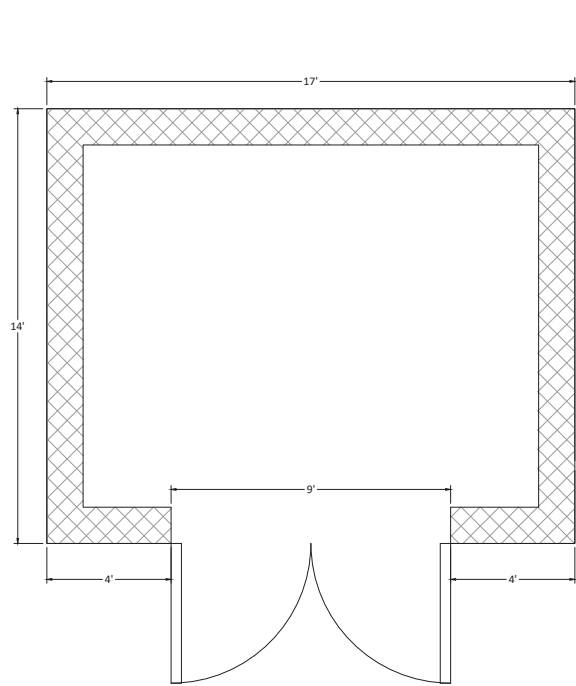
PROJECT NAME: ARCHIES BRAKE AND QUICK LUBE

NORTH LIBERTY, IA

CLIENT NAME: SWITCH DEVELOPMENT

SHEET NUMBER:
C4.00

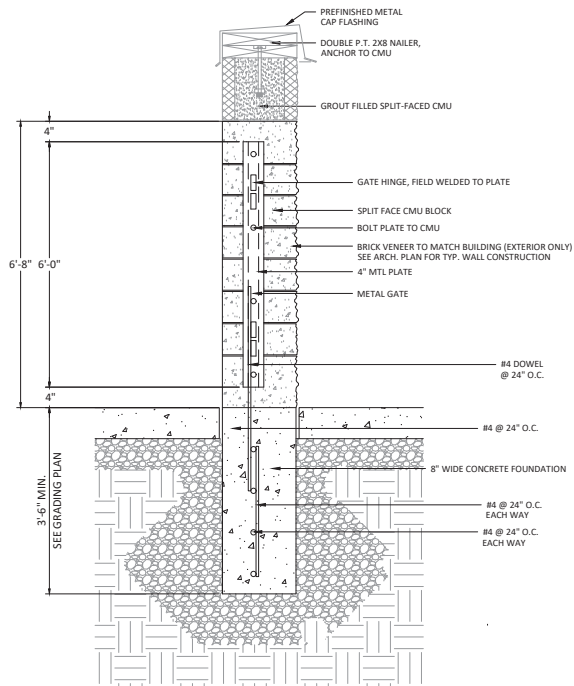




DUMPSTER ENCLOSURE

NOT TO SCALE

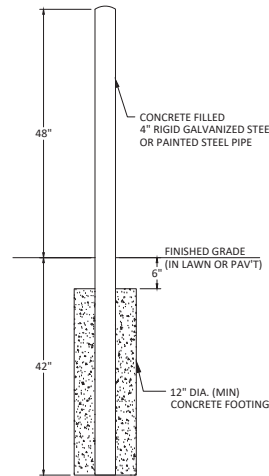
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GATE CONNECTION ELEVATION DETAIL

NOT TO SCALE

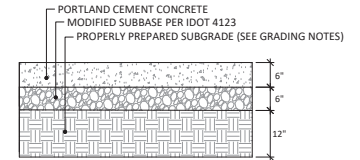
2



BOLLARD DETAIL

NOT TO SCALE

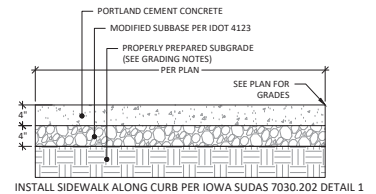
3



STANDARD DUTY PAVEMENT SECTION

NOT TO SCALE

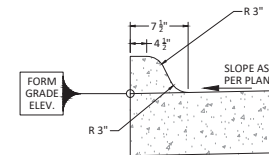
4



STANDARD PAVEMENT SIDEWALK SECTION

NOT TO SCALE

5



6" STANDARD CURB

NOT TO SCALE

6

NOT FOR CONSTRUCTION

CONSTRUCTION DETAILS

ARCHES BRAKE AND QUICK LUBE

NORTH LIBERTY, IA

SWITCH DEVELOPMENT

PROJECT NO. 220017

SHEET NUMBER

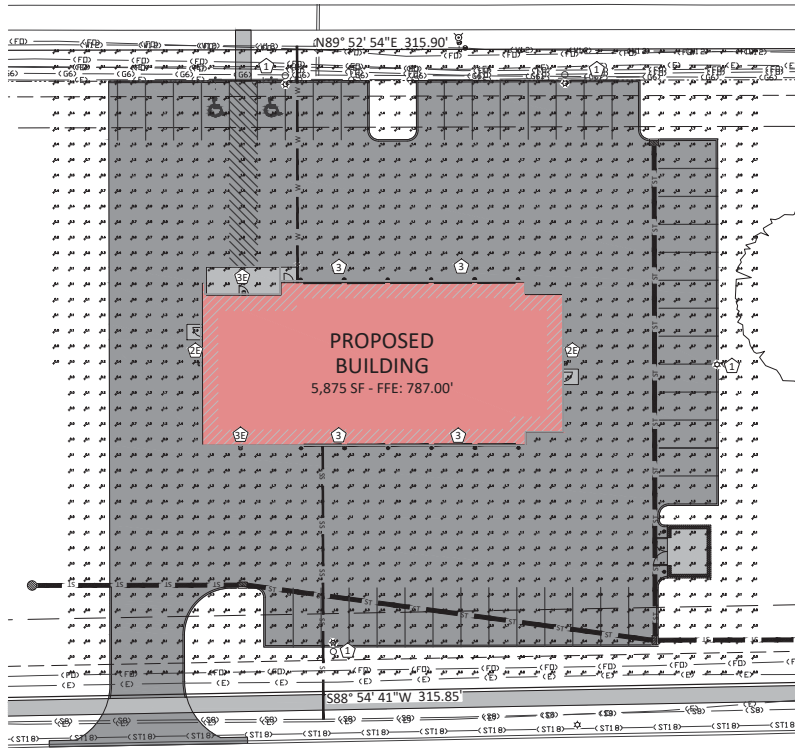
BOELK

C5.00

ENGINEER

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SANITARY
RISER

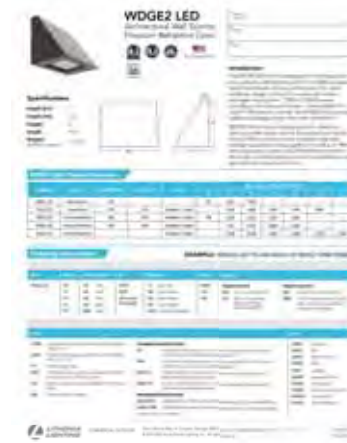
GENERAL NOTES

1. THE ELECTRICAL WORK SHALL BE INSTALLED IN ACCORDANCE WITH NEC 2017 AND ALL APPLICABLE LOCAL CODES.
2. CONTRACTOR SHALL FURNISH, INSTALL AND CONNECT ALL MATERIAL AND EQUIPMENT FOR THIS WORK UNLESS OTHERWISE NOTED.
3. EXACT LOCATION OF POLES TO BE COORDINATED IN THE FIELD WITH ENGINEER AND/OR OWNER.
4. INSTALL FOUNDATION FOR EACH POLE. SEE DETAIL ON THIS SHEET FOR ADDITIONAL INFORMATION.
5. CONNECT GROUND ROD TO GROUNDING CONDUCTOR ON LIGHTING CIRCUIT USING AN EXOTHERMIC TYPE CONNECTION.
6. ELECTRICAL CONTRACTOR TO VERIFY AVAILABLE VOLTAGE PRIOR TO ORDERING MATERIAL.
7. CONTROLS ARE NOT PROVIDED AS PART OF THIS WORK. COORDINATE EXTERIOR LIGHTING CONTROLS WITH ENGINEER/CONTRACTOR FOR I.R.L. BUILDING TWO.
8. INSTALL CONDUIT AND CONDUCTORS TO EACH FIXTURE. TYPE AND SIZE DEPENDS ON AVAILABLE VOLTAGE. CONFIRM AVAILABLE VOLTAGE WITH ENGINEER/CONTRACTOR FOR I.R.L. BUILDING TWO.

LIGHTING PLAN KEYNOTES

1. F1: PROPOSED LIGHT FIXTURE
MANUFACTURER: LITHONIA LIGHTING
MODEL #: RSX1 LED P3 40K R3 HS
MOUNTING: 25'
WATTAGE: 110
 2. F2: PROPOSED LIGHT FIXTURE
MANUFACTURER: LITHONIA LIGHTING
MODEL #: WDGE2 LED P3 40K 80CRI T3M
MOUNTING: 12'
WATTAGE: 32
 3. F3: PROPOSED LIGHT FIXTURE
MANUFACTURER: LITHONIA LIGHTING
MODEL #: WDGE1 LED P2 40K 80CRI VW
MOUNTING: 12'
WATTAGE: 15
4. DENOTES EGRESS FIXTURE WITH BATTERY BACKUP

TOTAL OUTDOOR LIGHT OUTPUT			
TAG	QUANTITY	LUMENS	TOTAL LUMENS
F-1	4	10,098	42,792
F-2	2	3,216	6,432
F-3	6	1,982	11,892
	TOTAL		61,116
	LUMENS PER NET ACRE (1.365)		44,774



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DRAWING LOG		REVISIONS	
REV	DESCRIPTION OF CHANGES	DATE	BY
1	CITY FIRST SUBMITTAL	03-28-2022	
2	RESPONSE TO CITY COMMENTS #1	03-23-2022	
3	RESPONSE TO CITY COMMENTS #2	03-31-2022	

PROJECT NAME: ARCHES BRAKE AND QUICK LUBE

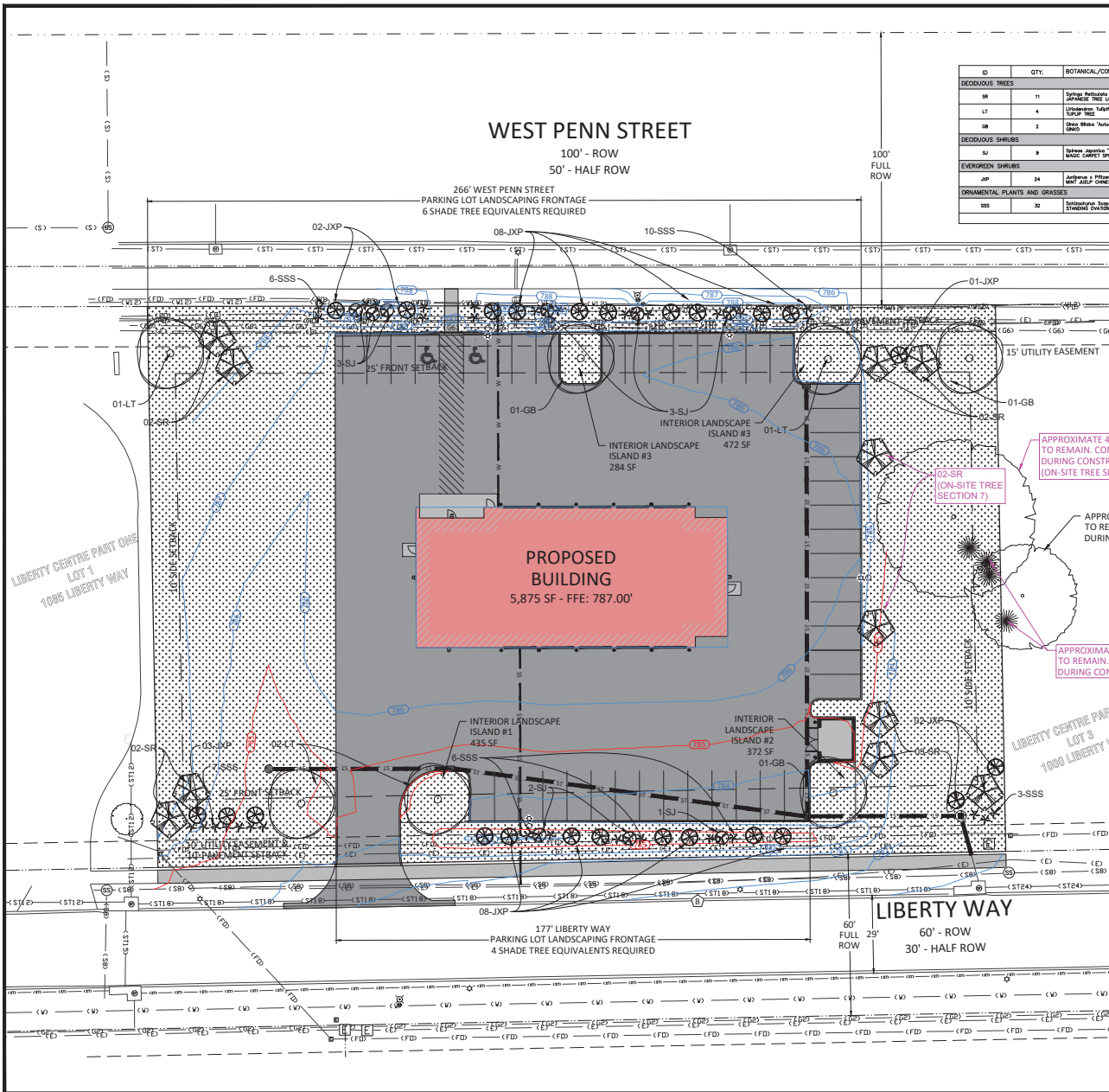
CITY NAME: NORTH LIBERTY, IA

CLIENT NAME: SWITCH DEVELOPMENT

PROJECT NO: 220017

DESIGNER: BOELK

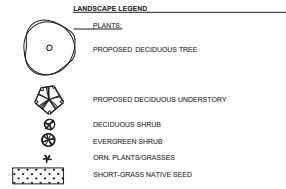
SHEET NUMBER: E1.00



ID	QTY.	BOTANICAL/COMMON NAME	MAT. SIZE	ROOT	SPACING	NOTES
DECIDUOUS TREES						
SH	11	Syringa Mollis JAPANESE TREE LILAC	30" H X 30" W	BB	SEE PLAN	MIN. OF 1.5" TRUNK DIAMETER AT 4" ABOVE GRADE
LT	4	Liquidambar styraciflua FLAMINGO PINE	60" H X 40" W	BB	SEE PLAN	MIN. OF 1.5" TRUNK DIAMETER AT 4" ABOVE GRADE
GB	2	Quercus macrocarpa WHITE OAK	40" H X 40" W	BB	SEE PLAN	MIN. OF 1.5" TRUNK DIAMETER AT 4" ABOVE GRADE
DECIDUOUS SHRUBS						
SJ	9	Shrub variety "Japanese" MADE CHINESE SPICE	2" H X 2" W	BB	SEE PLAN	MIN. OF 12" HEIGHT WHEN PLANTED
EVERGREEN SHRUBS						
SR	24	Juniperus x Procumbens "Blue Arrow" WEST CHINESE JUNIPER	4-5" H X 4-5" W	2 GAL.	SEE PLAN	MIN. OF 24" HEIGHT WHEN PLANTED
ORNAMENTAL PLANTS AND GRASSES						
SS	30	Schizanthus pinnatifidus "Standing Ovation" STANDING OVIATION LITTLE BLUE STEM	3-5" H X 1-2" W	2 GAL.	SEE PLAN	MIN. OF 24" HEIGHT WHEN PLANTED

LANDSCAPING NOTES

- PARKING LOT LANDSCAPING FRONTAGE SHOWN FOR WEST PENN STREET AND LIBERTY WAY AND REQUIRED SHADE TREE EQUIVALENTS CALCULATED. PROPOSED SHADE AND ORNAMENTAL TREES SHOWN FOR EACH FRONTAGE AND SET BACK FROM LANDSCAPING BERM AREA DUE TO EASEMENTS AND ABUNDANCE OF UNDERGROUND UTILITIES PRESENT. (CITY OF NORTH LIBERTY LANDSCAPING ORDINANCE SECTION 5.C)
- PARKING LOT LANDSCAPING FRONTAGE SHOWN FOR WEST PENN STREET INCLUDES PROPOSED LANDSCAPING THAT WOULD BE REQUIRED IF ADDITIONAL PARKING STALLS ADDED WEST OF THOSE PROPOSED WITH THIS SITE PLAN. (CITY OF NORTH LIBERTY LANDSCAPING ORDINANCE SECTION 5.C)
- ON-SITE TREES REQUIRED DUE TO BUILDING SIZE (1 SHADE TREE PER 2,000 - 5,875 SF / 2000 = 3 ON-SITE SHADE TREES) ARE PROVIDED AS SHOWN ON THE LANDSCAPE PLAN IN PURPLE. PROPOSED SHADE TREES LOCATED WITHIN ISLANDS TO FULFILL PARKING LOT LANDSCAPING REQUIREMENTS DUE TO EASEMENT AND UTILITY CONSTRAINTS.



LANDSCAPE PLANTING REQUIREMENTS

- 169.12 DESIGN STANDARDS**
- 169.13 LANDSCAPE REQUIREMENTS:**
- PARKING LOT SCREENING: SEE PLAN
 - PARKING LOT INTERNAL LANDSCAPING:
 - 41 SPACES SHOWN
 - 2% OF PARKING LOT FOR INTERIOR ISLAND
 - 3% OF 26,163 = 845 SF REQUIRED ISLAND AREA
 - 1.288 (450-1250) SF PROVIDED
 - SEEDS/SEED AS NOTED ON PLAN
 - SEEDS/SEED: 1 TREE / 2,000 SQ. FT. BLDG. FOOTPRINT
5,875 SQ. FT. / 2,000 = 3 REQ.
TREES PROVIDED
SCREENING: SEE PLAN
- SOD/SEED APPLICATION NOTES:**
- IF TURF GRASS LAWN SEED ALTERNATE IS USED IN LIEU OF SOD, ALL SEEDING APPLICATIONS SHALL BE PROVIDED BY UNITED SEEDS INC. OR APPROVED EQUAL.
 - ALL TURF GRASS LAWN AREAS, WHETHER SODDED OR SEED, UNLESS NOTED OTHERWISE, ARE TO BE PLANTED AND INSTALLED AS PER THE TECHNICAL BULLETIN FOR SOD GROWER II KENTUCKY BLUEGRASS BLENDED PROVIDED BY UNITED SEEDS, OR APPROVED EQUAL. TECHNICAL BULLETIN FOR SOD GROWER II KENTUCKY BLUEGRASS BLENDED CAN BE FOUND ON THE UNITED SEEDS WEBSITE: www.unitedseeds.com
 - ALL TURF GRASS SOD TO BE OF THE SAME PERFORMANCE QUALITY AND SPECIES TYPE OF THE "SOD GROWER II KENTUCKY BLUEGRASS BLENDED" PROVIDED BY UNITED SEEDS, OR APPROVED EQUAL. BLUEGRASS VARIETIES SHALL INCLUDE: "CONCERTO", "NU BLUE", "BRYNMAWR", AND "TANBURY".
- CONTACT INFO:**
- United Seeds, Inc.
1800 Dixon Ave., Suite A
Des Moines, IA 50319
Ph: 1-800-365-6674
Web: www.unitedseeds.com

SHORT-GRASS NATIVE SEED APPLICATION NOTES:

- NATIVE SEED GRASS SHALL BE "TUFFE BUCKWHEAT NATIVE MIXTURE" FROM UNITED SEEDS, INC. OR APPROVED EQUAL AND MIX WITH COVER CROP.
 - INSTALL PER SEED SOURCE SPECIFICATIONS.
- CONTACT INFORMATION:**
- UNITED SEEDS, INC.
1800 DIXON AVE., SUITE A
DES MOINES, IOWA
50319-3710
WWW.UNITSEEDS.COM



DRAWING LOG		
REV	DESCRIPTION OF CHANGES	DATE
1	CITY FIRST SUBMITTAL	02-28-2022
2	RESPONSE TO STAFF COMMENTS #1	03-23-2022
3	RESPONSE TO STAFF COMMENTS #2	03-31-2022

PROJECT NAME:	ARCHIES BRAKE AND QUICK LUBE
SHEET NAME:	LANDSCAPE PLAN
PROJECT NO.:	220017
DESIGNER/REVISIONAL:	BOELK

NOT FOR CONSTRUCTION

LIBERTY CENTRE PART ONE LOT 1

LIBERTY CENTRE PART ONE LOT 3

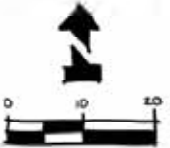
1006 LIBERTY WAY

1006 LIBERTY WAY

SWITCH DEVELOPMENT

ARCHIE'S LANDSCAPE RENDERING

WEST PENN STREET

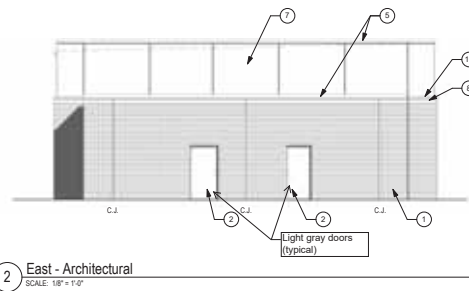
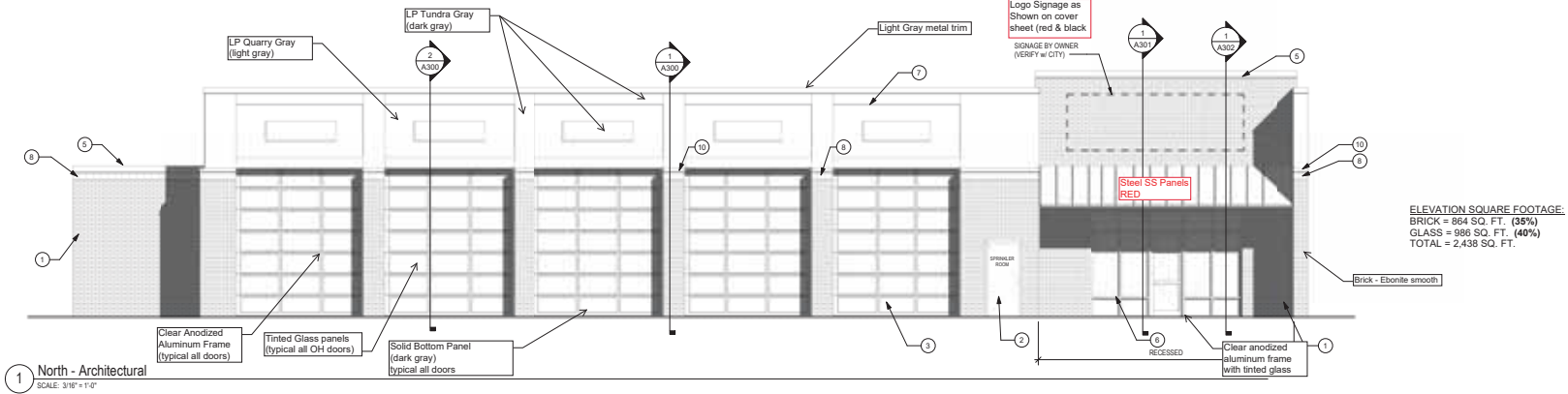


LIBERTY CENTRE PART ONE
LOT 1
1085 LIBERTY WAY

LIBERTY CENTRE PART ONE
LOT 3
1000 LIBERTY WAY

PROPOSED
BUILDING
1/4 ACRE 50' x 100' 100' 100'

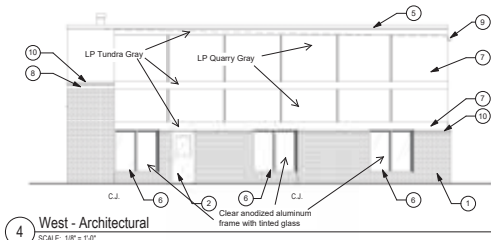
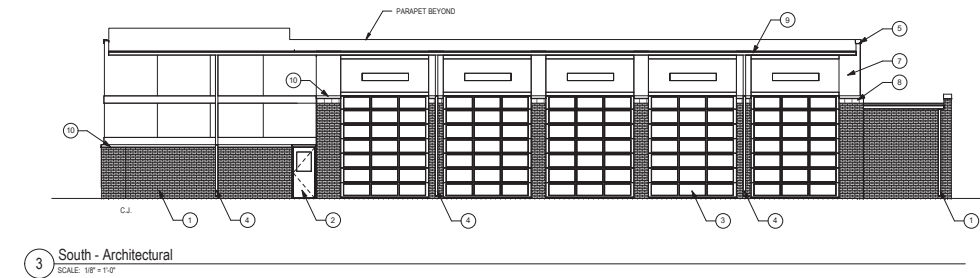
LIBERTY WAY



EXTERIOR ELEVATION KEYNOTES

- 1 EXTERIOR BRICK VENEER - EBNITE Smooth- (Dark Gray) Stone City Brick
 - 2 INSULATED H.M. DOOR - Light Gray
 - 3 GLASS OVERHEAD DOOR - Clear Anodized Aluminum Frame - Tinted Glass, Solid bottom panel - Dark Gray
 - 4 DOWNSPOUT - Light Gray
 - 5 PREFINISHED METAL CAP - Light Gray
 - 6 INSULATED GLAZING SET IN DARK - Clear Anodized Aluminum FRAME (VERIFY w/ OWNER)
 - 7 HARBORBOARD PANEL & TRIM - LP Smart Siding (Quarry Gray) with LP Smart Siding Trim (Tundra Gray)
 - 8 STONE ACCENT - Indiana Stone
 - 9 PREFINISHED GUTTER - Light Gray
 - 10 PREFINISHED FLASHING - Light Gray
- C.J. = CONTROL JOINT - MAX SPACING = 25' - 0"

ELEVATION SQUARE FOOTAGE TOTAL:
BRICK = 2,561 SQ. FT. (37%)
GLASS = 1,931 SQ. FT. (28%)
TOTAL = 6,898 SQ. FT.



606 14TH AVE. SW
CEDAR RAPIDS, IA 52404
319-365-1150

2435 KIMBERLY RD.
SUITE 2405
BETHTOON, IA 52722
563-359-3117

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REVISIONS

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ARCHIE'S BRAKE & QUICK LUBE

1040 LIBERTY WAY
NORTH LIBERTY, IA 52317
APEX CONSTRUCTION

SHEET TITLE
EXTERIOR ELEVATIONS

DRAWN BY: PDW
CHECKED BY: DMQ/KRS
APPROVED BY: DMQ/KRS
JOB DATE: 2022.03.21
JOB NO: 22-133

ISSUED FOR PERMIT
2022.03.21

PAGE NO.
A200



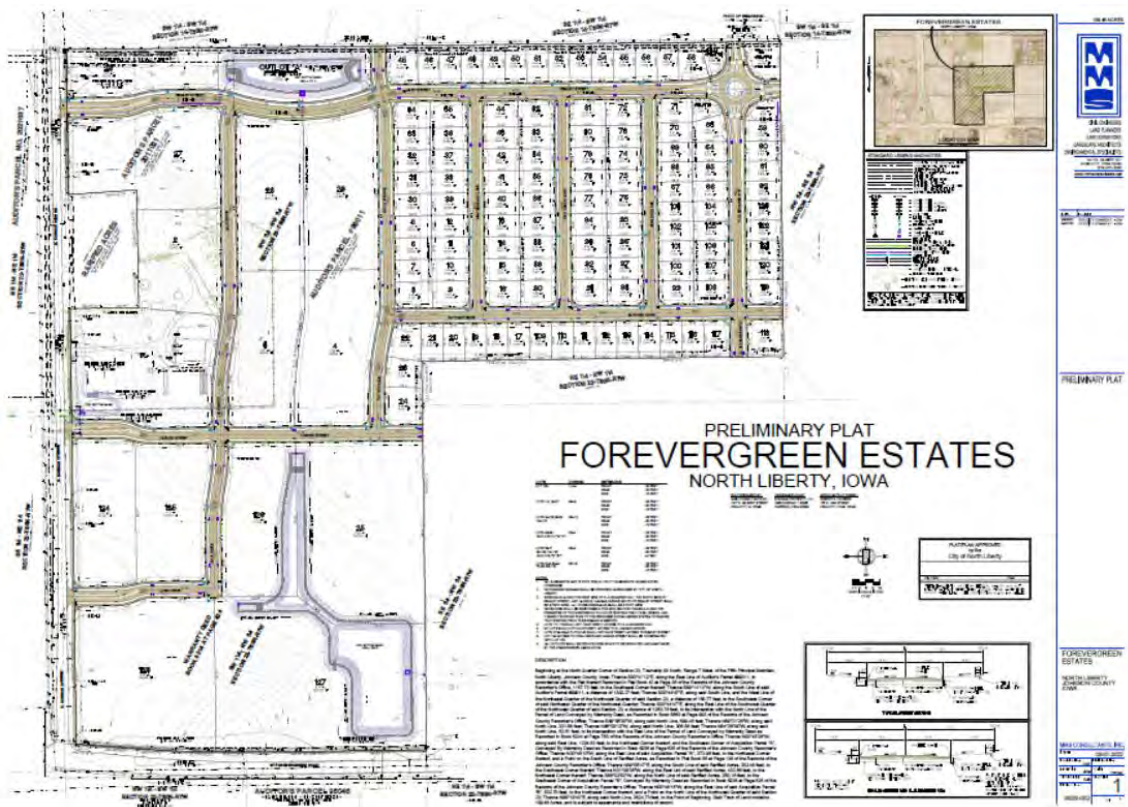
To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of Bowman Property, LLC to approve a Preliminary Subdivision Plat for a 129 lot subdivision on 109.46 acres, more or less, on property located at the northeast corner of West Forevergreen Road and South Kansas Avenue.**

North Liberty City staff has reviewed the subject submission, and offer comments presented in this memo. The staff review team includes the following personnel:

Ryan Heiar, City Administrator
Tracey Mulcahey, Assistant City Administrator
Grant Lientz, City Attorney
Tom Palmer, City Building Official
Kevin Trom, City Engineer
Ryan Rusnak, Planning Director

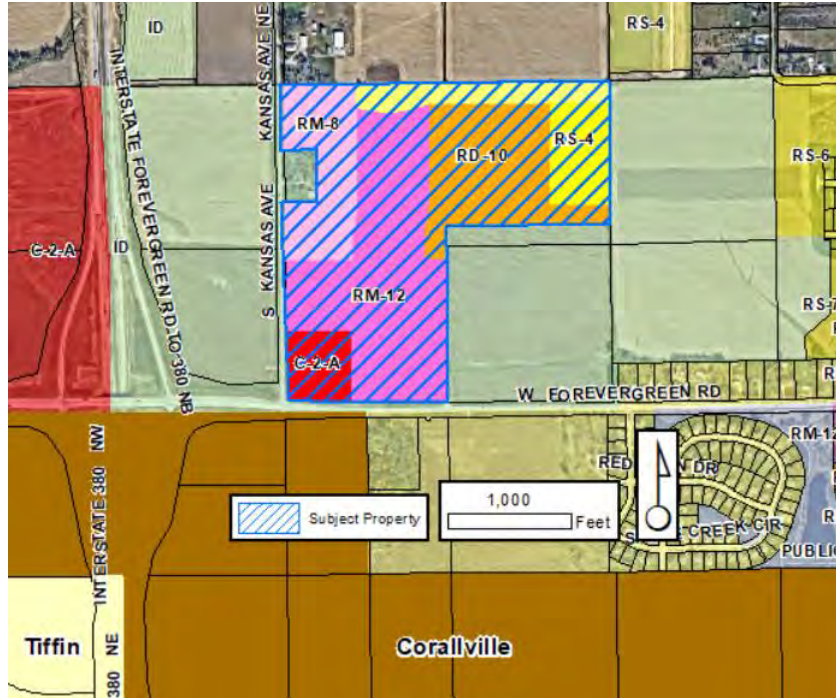
1. Request Summary:

The Preliminary Plat proposes 129 lots and related infrastructure on 109.46 acres.



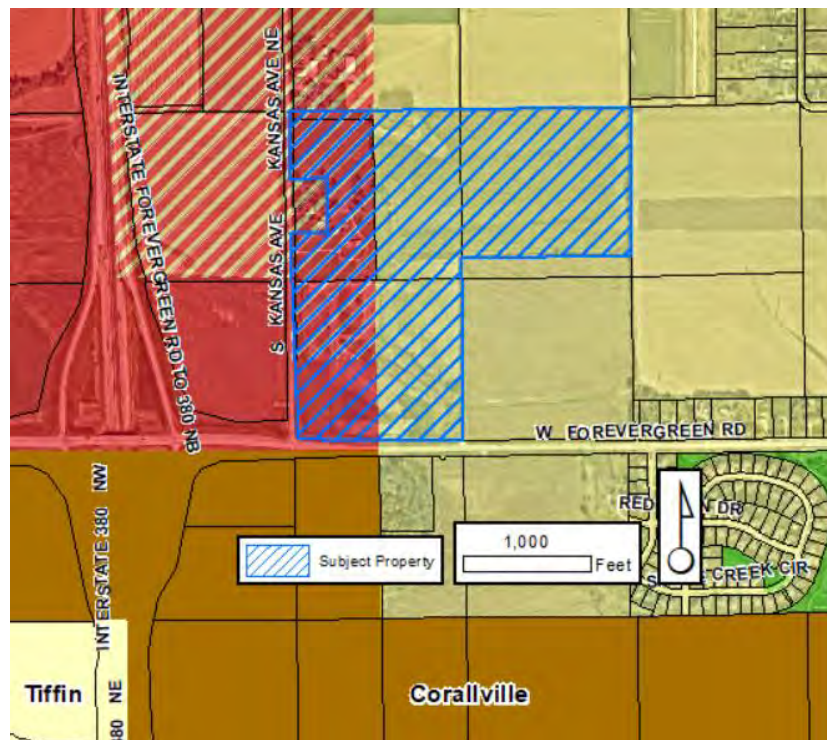
2. Current Zoning:

The property is currently zoned RS-4 – Single-Unit Residence District, RS-6 – Single-Unit Residence District, RD-10 – Two-Unit Residence District, RM-8, Multi-Unit Residence District, RM-12 Multi-Unit Residence District and C-2-A Highway Commercial District.



3. Comprehensive Plan Future Land Use Map Designation:

Residential, Commercial and Commercial with Residential.



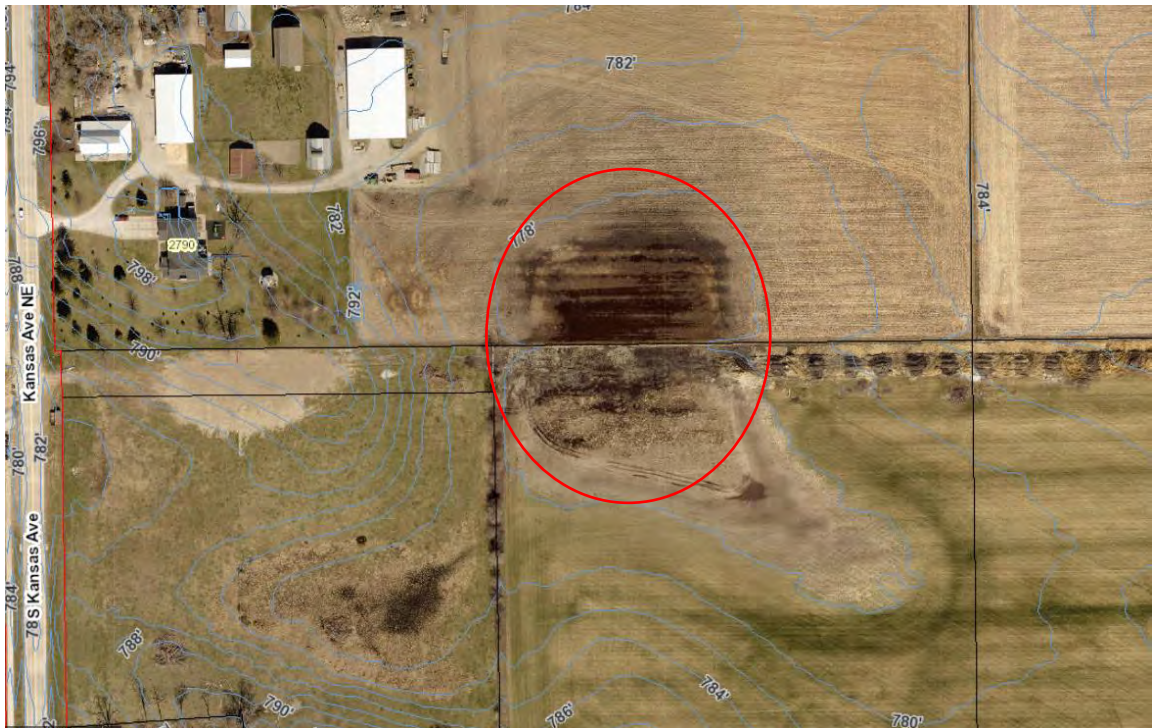
It appears that the Commercial with Residential designation was part of a more recent Future Land Use Map update. It is staff's understanding that the designation was to encourage mixed commercial/residential development.

It is staff's opinion that the preliminary plat, which proposes residential and commercial development would achieve consistency with the Future Land Use designations, which are Residential, Commercial and Residential with Commercial. If approved, staff would initiate a Land Use amendment for the portion of the property designated Commercial with Residential so that the Land Use Map reflects entirely residential development.

4. Public Input:

Letters were sent to property owners within 200 feet of the subject property and notice was posted on the City's website regarding the March 21, 2022 good neighbor meeting, which has held in person. Four area residents attended and there general concerns about the proposed development.

The adjacent property owners to the north expressed an important concern regarding drainage. There is a low spot between the two properties, which does not have an immediate outfall. Please see discussion regarding this concern in the approval standards section.



The other concern expressed by the property owners to the north was maintenance of the fence along the common property line. It is staff's opinion that the 10' landscape easement (and the sanitary sewer force main easement) should be unobstructed by homeowner fences and that the homeowners' association should be given free access

and maintenance responsibilities to the south side of the fence. This would need to be articulated in a future recorded landscape easement.

5. Approval Standards:

Section 180.11(3)(A) of the North Liberty Code of Ordinances sets forth the preliminary subdivision plat submittal requirements and review (Ordinance language in *italics* and staff analysis in **bold**).

3. *Preliminary Subdivision Plat Submittal Requirements and Review. The preliminary plat, in general, contains more information than the final plat, so that the subdivider and the City can ensure conformance with codes, master facility plans, and good planning and engineering practices. Though the preliminary plat is not recorded, it is approved by resolution of the City Council, and conditions for approval of the plat shall be addressed on any final plats of the same area.*

A. *Preliminary Plat Contents. The application shall include a preliminary plat of the subdivision drawn to a scale of one inch to one hundred feet minimum, and shall show:*

(1) *A location map to provide spatial reference, showing the outline of the area to be subdivided, existing streets and corporate limits in the vicinity, a north arrow and scale or note stating "not to scale," and other information that might help clarify where the plat is located as well as its surroundings and size relative to other City features;*

This has been provided on the Preliminary Plat.

(2) *Name of proposed subdivision and date;*

This has been provided on the Preliminary Plat.

(3) *Legal description and acreage;*

This has been provided on the Preliminary Plat.

(4) *Name and address of owner;*

This has been provided on the Preliminary Plat.

(5) *Names of the persons preparing the plat, owner's attorney, representative or agent, if any;*

This has been provided on the Preliminary Plat.

(6) *Existing and proposed zoning district classification of all land within the proposed subdivision and within about 200 feet of the subdivision;*

Staff is not requiring this information on the Preliminary Plat.

(7) *North point and graphic scale;*

This has been provided on the Preliminary Plat.

(8) *Contours at two-foot intervals or less, both existing and as generally proposed (subject to more refinement in subsequent construction plans);*

This has been provided on the Preliminary Plat.

(9) *Building setback lines as required by the current or proposed zoning district classifications;*

This has been provided on the Preliminary Plat.

(10) *The approximate boundaries of areas of known flood levels or floodplains, areas covered by water, wooded areas, floodways, and all open channel drainage ways;*

This has been provided on the Preliminary Plat.

(11) *Locations, names, and dimensions of existing lot lines, streets, public utilities, water mains, sewers, drainpipes, culverts, watercourses, bridges, railroads and buildings within in the proposed subdivision and within about 200 feet of the subdivision;*

This has been provided on the Preliminary Plat.

(12) *Layout of proposed blocks, if used, and lots, including the dimension of each lot, and the lot and block number in numerical order;*

This has been provided on the Preliminary Plat.

(13) *Layout and dimensions of proposed streets, sidewalks, trails, alleys, utility and other easements, parks and other open spaces or reserved areas;*

This has been provided on the Preliminary Plat.

(14) *Grades of proposed streets and alleys;*

This has been provided on the Preliminary Plat.

(15) *A cross-section of the proposed streets showing the roadway locations, the type of curb and gutter, the paving, and sidewalks to be installed;*

This has been provided on the Preliminary Plat.

(16) *The layout of proposed water mains and sanitary sewer systems;*

This has been provided on the Preliminary Plat.

(17) *The drainage of the land, including proposed storm sewers, ditches, culverts, bridges and other structures;*

This has been provided on the Preliminary Plat.

(18) *Stormwater management facilities when applicable;*

This has been provided on the Preliminary Plat.

(19) *A signed certificate of the Johnson County Auditor for the subdivision name;*

This information has been provided.

(20) Other special details or features that may be proposed or required.
None required.

6. Additional Considerations:

Proposed Transportation Network.

Remley Street (northernmost east/west street in the concept plan) is ultimately planned from South Jones Boulevard to South Kansas Avenue. This is a planned collector street and would have an 8' wide path.

Alexander Way (easternmost north/south street in the concept plan) is ultimately planned from West Forevergreen Road to West Penn Street. This is a planned collector street and would have an 8' wide path.

Staff requested and the applicant agreed to provide a at the intersection of these two collector streets. This would be a compact roundabout similar to Penn and Front Streets.

The rest of the development is generally laid out on a grid with the exception of the multi-family family development adjacent to South Kansas Avenue.

There would be an 8' wide path along the east side of South Kansas Avenue to West Forevergreen Road. It is staff's opinion the future signalized intersection in this location would be much safer to cross.

Notably, street names were chosen to pay homage to the members of the Sentman/Rarick family who, until recently, farmed the property since the latter part of the 1800s.

Stormwater Management.

As mentioned, there is a low spot between the subject property and the abutting property the north, which does not have an immediate outfall. The applicant is proposing to provide more detention than what would be normally required in order to send its stormwater to the south basin. A storm event above the 1% annual storm would overflow onto the property to the north.

Staff generally agrees with this solution, but will need to review construction plans to ensure the solution is adequate.

The development would be required to explore for and address any underground field tiles along this and adjacent property lines under common ownership.

Other Concerns.

It is staff that there should be no direct access from Lot 124 to South Kansas Avenue. Staff shares draws attention to Lot 127, however, does not wish to restrict access without knowing the future development of the lot.

The outlots adjacent to the roundabout would be a concern if not properly maintained. However, this the homeowners' association would be maintaining the landscaping easement and fence along the north perimeter property line, this is less of a concern.

7. Staff Recommendation:

Findings:

1. The preliminary plat, which proposes residential and commercial development would achieve consistency with the Future Land Use designations, which are Residential, Commercial and Residential with Commercial. If approved, staff will initiate a Land Use amendment for the portion of the property designated Commercial with Residential so that the Land Use Map reflects entirely residential development.
2. The preliminary plat, with conditional recommended by City staff, would achieve consistency with Section 180.11(3)(A) and 180.12 of the North Liberty Code of Ordinances, which sets forth the preliminary subdivision plat submittal requirements and design standards, respectively.

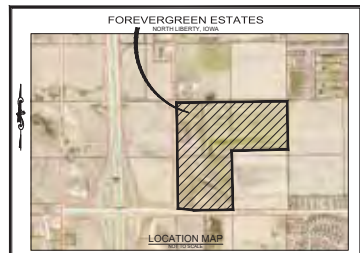
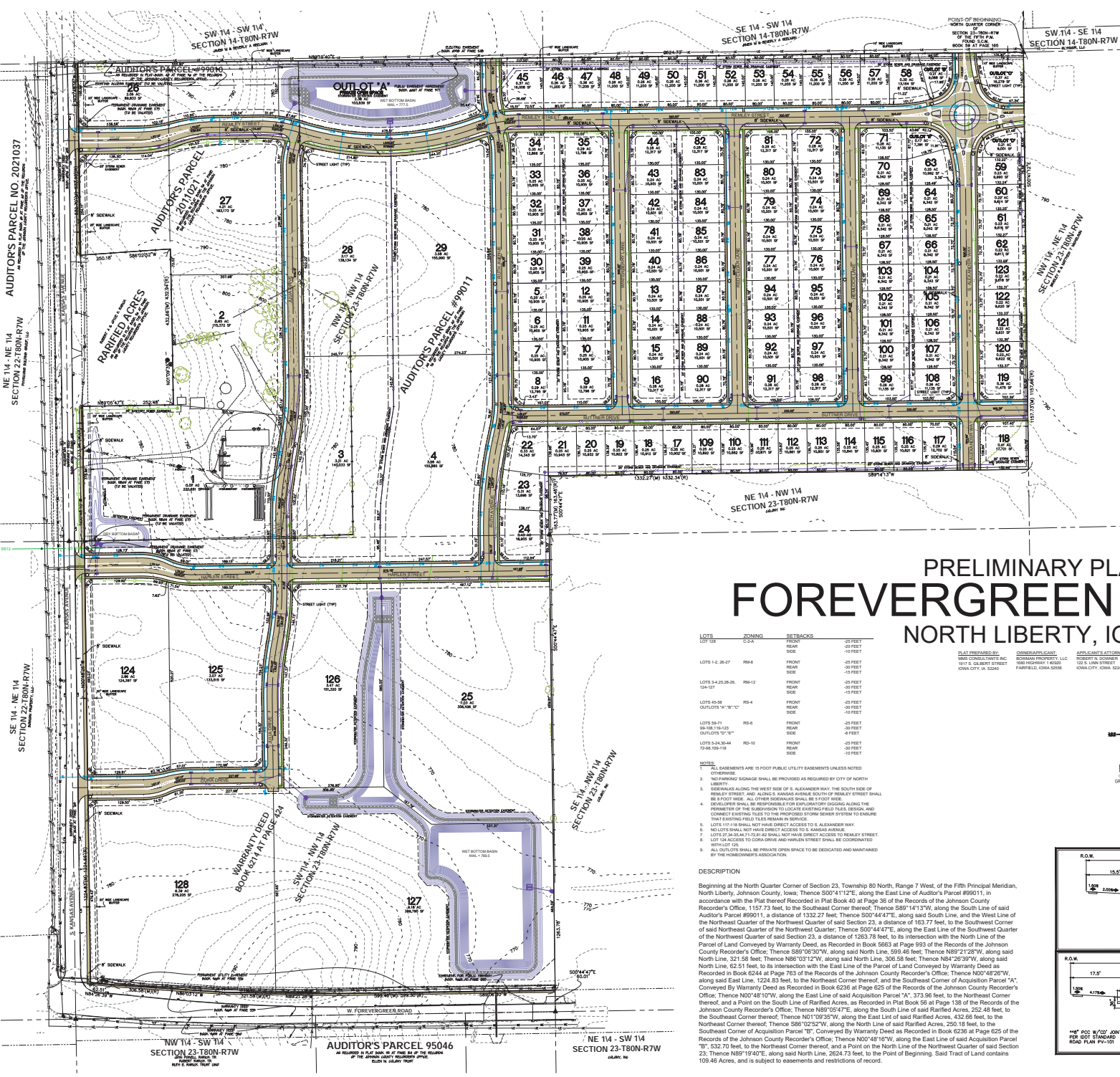
Recommendation:

Staff recommends the Planning Commission accept the two listed findings and forward the request of Bowman Property, LLC to approve a Preliminary Subdivision Plat for a 129 lot subdivision on 109.46 acres, more or less, on property located at the northeast corner of West Forevergreen Road and South Kansas Avenue to the City Council with a recommendation for approval subject to the following conditions:

1. That the 10' landscape easement and the sanitary sewer force main easement be left unobstructed by fences (other than the fence along the north perimeter property line) and that the homeowners' association be responsible for maintenance of the area within the landscape easement and the south side of the north perimeter fence and that said maintenance responsibilities be articulated in a recorded landscape easement at the time of final plat; and
2. That the preliminary plat be revised to include the restriction that Lot 124 shall have no direct access to South Kansas Avenue.

Suggested motion:

I move that the Planning Commission accept the two listed conditions and forward the Preliminary Plat to the City Council with a recommendation for approval subject to the two conditions listed by City Staff.



STANDARD LEGEND AND NOTES

PROPERTY A/W BOUNDARY LINES	PROPERTY A/W BOUNDARY LINES
CONVEYANCE SECTION LINES	CONVEYANCE SECTION LINES
EXISTING RIGHT-OF-WAY LINES	EXISTING RIGHT-OF-WAY LINES
EXISTING CENTER LINES	EXISTING CENTER LINES
LOT LINES	LOT LINES
EXISTING EASEMENTS	EXISTING EASEMENTS
EXISTING EASEMENTS	EXISTING EASEMENTS

NOTES:

- 1. THE ACTUAL SIZE AND LOCATION OF ALL PROPOSED FACILITIES SHALL BE DETERMINED BY THE ENGINEER'S FIELD SURVEY AND SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
- 2. THE ENGINEER'S FIELD SURVEY SHALL BE CONDUCTED IN ACCORDANCE WITH THE IOWA SURVEYING BOARD RULES AND REGULATIONS.
- 3. THE ENGINEER'S FIELD SURVEY SHALL BE CONDUCTED IN ACCORDANCE WITH THE IOWA SURVEYING BOARD RULES AND REGULATIONS.

LOTS	ZONING	SETBACKS
LOTS 1-2, 20-27	RM-6	FRONT: 35 FEET REAR: 35 FEET SIDE: 35 FEET
LOTS 3-4, 28-30	RM-12	FRONT: 35 FEET REAR: 35 FEET SIDE: 35 FEET
LOTS 45-58	RS-4	FRONT: 35 FEET REAR: 35 FEET SIDE: 35 FEET
LOTS 59-71	RS-6	FRONT: 35 FEET REAR: 35 FEET SIDE: 35 FEET
LOTS 72-84	RS-10	FRONT: 35 FEET REAR: 35 FEET SIDE: 35 FEET

NOTES:

1. ALL EASEMENTS ARE 10 FOOT PUBLIC UTILITY EASEMENTS UNLESS NOTED OTHERWISE.
2. NO PARKING OR STORAGE SHALL BE PROVIDED AS REQUIRED BY CITY OF NORTH LIBERTY.
3. SETBACKS ALONG THE WEST SIDE OF S. ALEXANDER WAY, THE SOUTH SIDE OF REMLEY STREET, AND ALONG S. KANGA AVENUE SOUTH OF REMLEY STREET SHALL BE 10 FOOT MIN. ALL OTHER SETBACKS SHALL BE 35 FOOT MIN.
4. DEVELOPER SHALL BE RESPONSIBLE FOR OBTAINING NECESSARY PERMITS AND FOR THE PERMITTING PROCESS TO LOCATE EXISTING FIELD TILE DESIGN AND EXISTING FIELD TILES REMAIN IN SERVICE.
5. LOTS 114-118 SHALL NOT HAVE DIRECT ACCESS TO S. ALEXANDER WAY.
6. NO LOTS SHALL HAVE DIRECT ACCESS TO S. KANGA AVENUE.
7. LOTS 27-34 AND 71-74 SHALL NOT HAVE DIRECT ACCESS TO REMLEY STREET.
8. LOT 114 ACCESS TO CORA DRIVE AND HAVEN STREET SHALL BE COORDINATED WITH LOTS 125.
9. ALL OUTLOTS SHALL BE MAINTAIN OPEN SPACE TO BE DESIGNATED AND MAINTAINED BY THE HOMEOWNER'S ASSOCIATION.

DESCRIPTION

Beginning at the North Quarter Corner of Section 23, Township 80 North, Range 7 West, of the Fifth Principal Meridian, North Liberty, Johnson County, Iowa; Thence S00°41'12"E, along the East Line of Auditor's Parcel #90011, in accordance with the Plat thereof Recorded in Plat Book 40 at Page 36 of the Records of the Johnson County Recorder's Office, 1157.73 feet, to the Southwest Corner thereof; Thence S89°14'13"W, along the South Line of said Auditor's Parcel #90011, a distance of 1332.27 feet; Thence S00°44'47"E, along said South Line, and the West Line of the Northeast Quarter of the Northwest Quarter of said Section 23, a distance of 163.77 feet, to the Southwest Corner of said Northeast Quarter of the Northwest Quarter; Thence S90°44'47"E, along the East Line of the Southwest Quarter of the Northwest Quarter of said Section 23, a distance of 1263.78 feet, to its intersection with the North Line of the Parcel of Land Conveyed by Warranty Deed, as Recorded in Book 5663 at Page 593 of the Records of the Johnson County Recorder's Office, Thence S89°00'30"W, along said North Line, 599.46 feet; Thence N85°21'25"W, along said North Line, 321.58 feet; Thence N86°03'12"W, along said North Line, 306.58 feet; Thence N84°26'39"W, along said North Line, 625.51 feet, to its intersection with the East Line of the Parcel of Land Conveyed by Warranty Deed as Recorded in Book 6244 at Page 763 of the Records of the Johnson County Recorder's Office, Thence N00°46'20"W, along said East Line, 1224.83 feet, to the Northeast Corner thereof, and the Southeast Corner of Acquisition Parcel "A", Conveyed by Warranty Deed as Recorded in Book 6235 at Page 625 of the Records of the Johnson County Recorder's Office, Thence N00°46'19"W, along the East Line of said Acquisition Parcel "A", 373.56 feet, to the Northeast Corner thereof, and a Point on the South Line of said Acquisition Parcel "A", as Recorded in Plat Book 56 at Page 138 of the Records of the Johnson County Recorder's Office, Thence N89°03'47"E, along the South Line of said Acquisition Parcel, 252.48 feet, to the Southeast Corner thereof; Thence N01°00'35"W, along the East Line of said Acquisition Parcel, 432.66 feet, to the Northeast Corner thereof; Thence S88°02'52"W, along the North Line of said Acquisition Parcel, 250.18 feet, to the Southeast Corner of Acquisition Parcel "B", Conveyed by Warranty Deed as Recorded in Book 6235 at Page 625 of the Records of the Johnson County Recorder's Office, Thence N00°46'19"W, along the East Line of said Acquisition Parcel "B", 532.70 feet, to the Northeast Corner thereof, and a Point on the North Line of the Northwest Quarter of said Section 23, Thence N89°19'07"E, along said North Line, 2024.73 feet, to the Point of Beginning. Said Tract of Land contains 109.46 Acres, and is subject to easements and restrictions of record.

PRELIMINARY PLAT FOR EVERGREEN ESTATES NORTH LIBERTY, IOWA

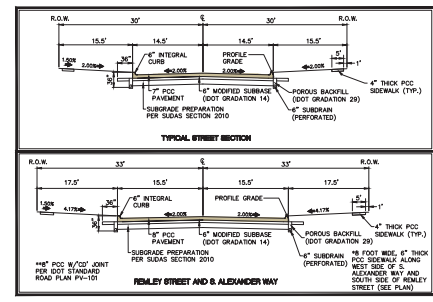


PLAT/PLAN APPROVED
by the
City of North Liberty

City Clerk _____ Date _____

City Engineer _____ Date _____

City Attorney _____ Date _____



109.46 ACRES

CIVIL ENGINEERS
LAND PLANNERS
LANDSCAPE ARCHITECTS
ENVIRONMENTAL SPECIALISTS

1917 S. GILBERT ST.
JOHN CITY, IOWA 52245
(515) 951-6262
WWW.MMSCONSULTANTS.COM

Date: 03-01-2022

Revision: PER CITY COMMENT - JCM

PRELIMINARY PLAT

FOREVERGREEN ESTATES

NORTH LIBERTY, JOHNSON COUNTY, IOWA

MMS CONSULTANTS, INC.

Designed by: JCM

Drawn by: JCM

Checked by: JCM

Project No: 1

9828-002

SITE GRADING CONCEPT

NORTH LIBERTY
JOHNSON COUNTY
IOWA

Date: 03.01.2022

Designed by:	Field Book No:
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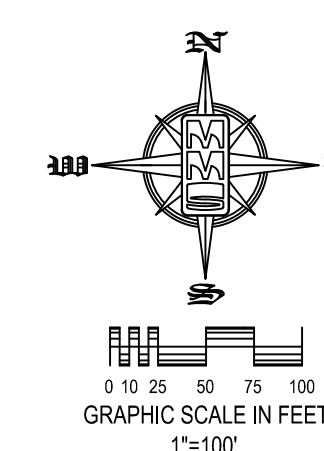
CAT	
Drawn by:	Scale:

JDM	1"=100'
Checked by:	Sheet No:

Project No: KJB

[illegible]

9828-002	of: 1
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<u>PLAT PREPARED BY:</u>	<u>OWNER/APPLICANT:</u>	<u>APPLICANT'S ATTORNEY:</u>
MMS CONSULTANTS INC.	BOWMAN PROPERTY, LLC	ROBERT N. DOWNER
1917 S. GILBERT STREET	1680 HIGHWAY 1 #2920	122 S. LINN STREET
IOWA CITY, IA 52240	FAIRFIELD, IOWA 52556	IOWA CITY, IOWA 52240



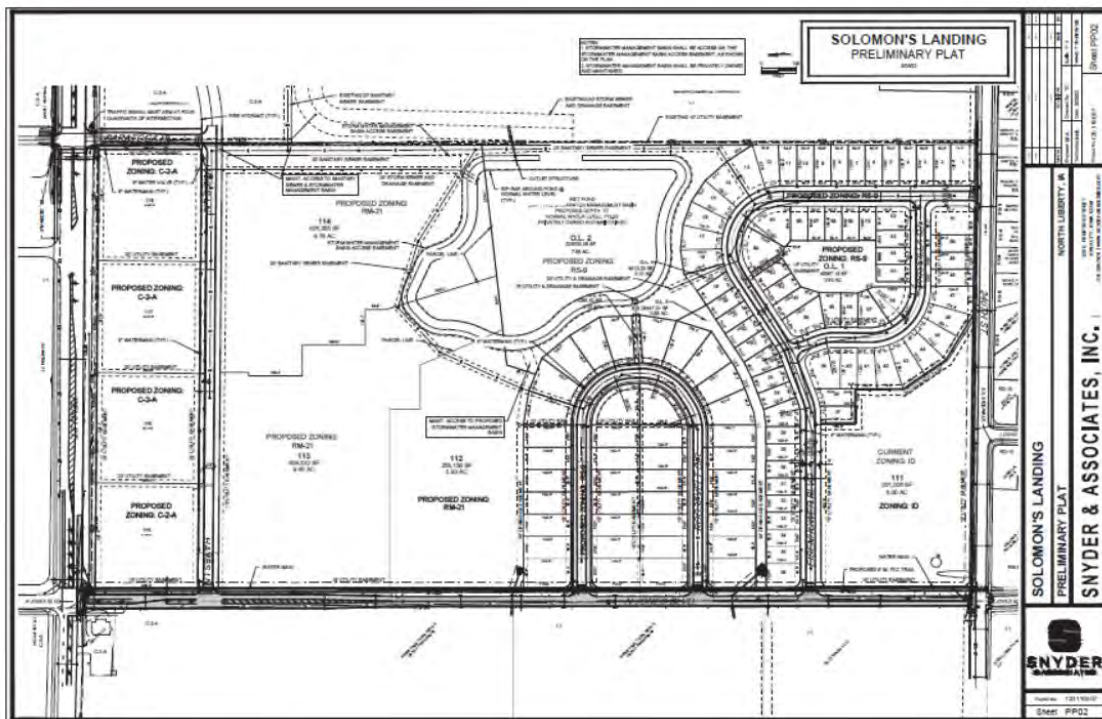
To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of Solomon Holdings, LLC to approve a Preliminary Subdivision Plat for a 118 lot subdivision on 76.97 acres, more or less, on property located on the north side of West Penn Street west of the North Jones Boulevard as extended northerly from West Penn Street to 240th Street.**

North Liberty City staff has reviewed the subject submission, and offer comments presented in this memo. The staff review team includes the following personnel:

Ryan Heiar, City Administrator
Tracey Mulcahey, Assistant City Administrator
Grant Lientz, City Attorney
Tom Palmer, City Building Official
Kevin Trom, City Engineer
Ryan Rusnak, Planning Director

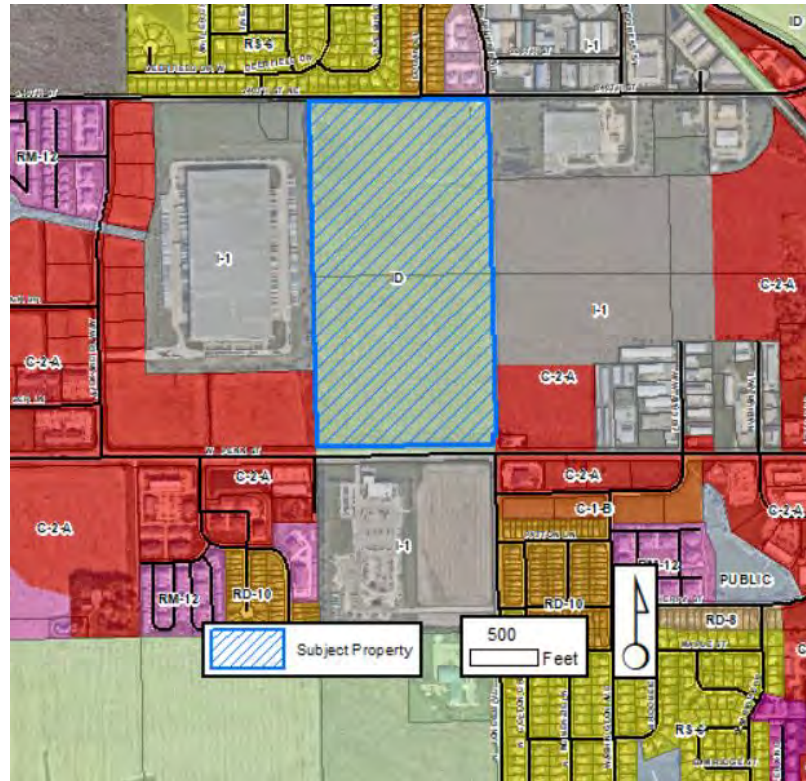
1. Request Summary:

The Preliminary Plat proposes 118 lots and related infrastructure on 76.97 acres.



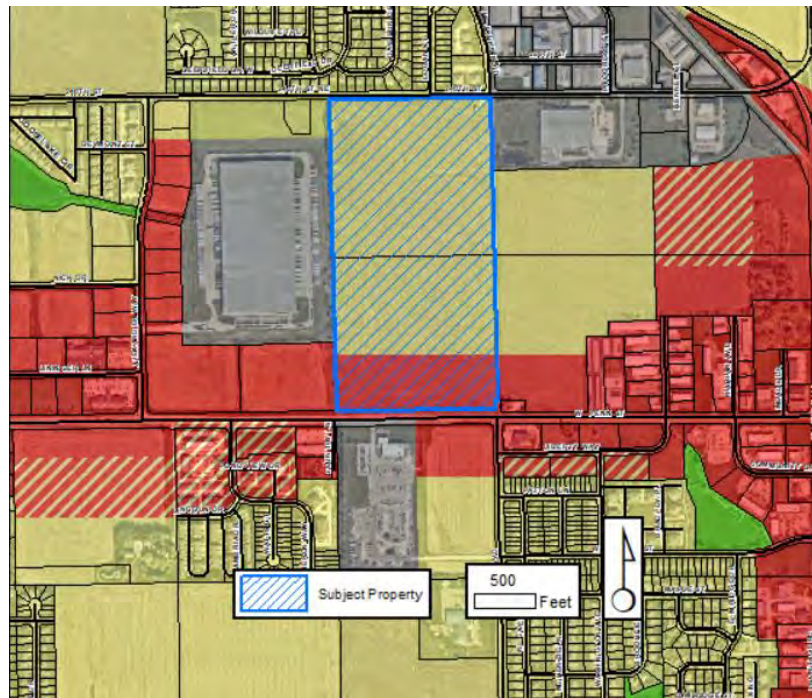
2. Current Zoning:

The property is currently zoned ID Interim Development District. The applicant is requesting a rezoning to RS-9 Single-Unit Residence District on 34.56 acres, RM-21 Multiple-Unit Residence District on 26.85 acres and C-2-A Highway Commercial District on 24.87 acres. It is anticipated that the preliminary plat would be considered at the same meeting as City Council's final approval of the rezoning.



Proposed zoning districts.

3. Comprehensive Plan Future Land Use Map Designation: Residential and Commercial.



The proposed preliminary plat would achieve consistency with the Future Land Use Map.

4. Public Input:

The January 27, 2022 good neighbor meeting was held in conjunction with the rezoning request.

Approximately 15 people attended the meeting and had general questions about the proposed development. The applicant did not request a rezoning at the northeast corner of the property due to the concerns about compatibility and not having a potential end-user at that location. Also, one user of the City's multi-use trail networks expressed a concern over the number of vehicular access points to North Jones Boulevard.

5. Approval Standards:

Section 180.11(3)(A) of the North Liberty Code of Ordinances sets forth the preliminary subdivision plat submittal requirements and review (Ordinance language in *italics* and staff analysis in **bold**).

3. *Preliminary Subdivision Plat Submittal Requirements and Review. The preliminary plat, in general, contains more information than the final plat, so that the subdivider and the City can ensure conformance with codes, master facility plans, and good planning and engineering practices. Though the preliminary plat is not recorded, it is approved by resolution of the City Council, and conditions for approval of the plat shall be addressed on any final plats of the same area.*

A. *Preliminary Plat Contents. The application shall include a preliminary plat of the subdivision drawn to a scale of one inch to one hundred feet minimum, and shall show:*

(1) A location map to provide spatial reference, showing the outline of the area to be subdivided, existing streets and corporate limits in the vicinity, a north arrow and scale or note stating "not to scale," and other information that might help clarify where the plat is located as well as its surroundings and size relative to other City features;

This has been provided on the Preliminary Plat.

(2) Name of proposed subdivision and date;

This has been provided on the Preliminary Plat.

(3) Legal description and acreage;

This has been provided on the Preliminary Plat.

(4) Name and address of owner;

This has been provided on the Preliminary Plat.

(5) Names of the persons preparing the plat, owner's attorney, representative or agent, if any;

This has been provided on the Preliminary Plat.

(6) Existing and proposed zoning district classification of all land within the proposed subdivision and within about 200 feet of the subdivision;

Staff is not requiring this information on the Preliminary Plat.

(7) North point and graphic scale;

This has been provided on the Preliminary Plat.

(8) Contours at two-foot intervals or less, both existing and as generally proposed (subject to more refinement in subsequent construction plans);

This has been provided on the Preliminary Plat.

(9) Building setback lines as required by the current or proposed zoning district classifications;

This has been provided on the Preliminary Plat.

(10) The approximate boundaries of areas of known flood levels or floodplains, areas covered by water, wooded areas, floodways, and all open channel drainage ways;

This has been provided on the Preliminary Plat.

(11) Locations, names, and dimensions of existing lot lines, streets, public utilities, water mains, sewers, drainpipes, culverts, watercourses, bridges, railroads and buildings within in the proposed subdivision and within about 200 feet of the subdivision;

This has been provided on the Preliminary Plat.

(12) Layout of proposed blocks, if used, and lots, including the dimension of each lot, and the lot and block number in numerical order;

This has been provided on the Preliminary Plat.

(13) Layout and dimensions of proposed streets, sidewalks, trails, alleys, utility and other easements, parks and other open spaces or reserved areas;

This has been provided on the Preliminary Plat.

(14) Grades of proposed streets and alleys;

This has been provided on the Preliminary Plat.

(15) A cross-section of the proposed streets showing the roadway locations, the type of curb and gutter, the paving, and sidewalks to be installed;

This has been provided on the Preliminary Plat.

(16) The layout of proposed water mains and sanitary sewer systems;

This has been provided on the Preliminary Plat.

(17) The drainage of the land, including proposed storm sewers, ditches, culverts, bridges and other structures;

This has been provided on the Preliminary Plat.

(18) Stormwater management facilities when applicable;

This has been provided on the Preliminary Plat.

(19) A signed certificate of the Johnson County Auditor for the subdivision name;

This information has been provided.

(20) Other special details or features that may be proposed or required.

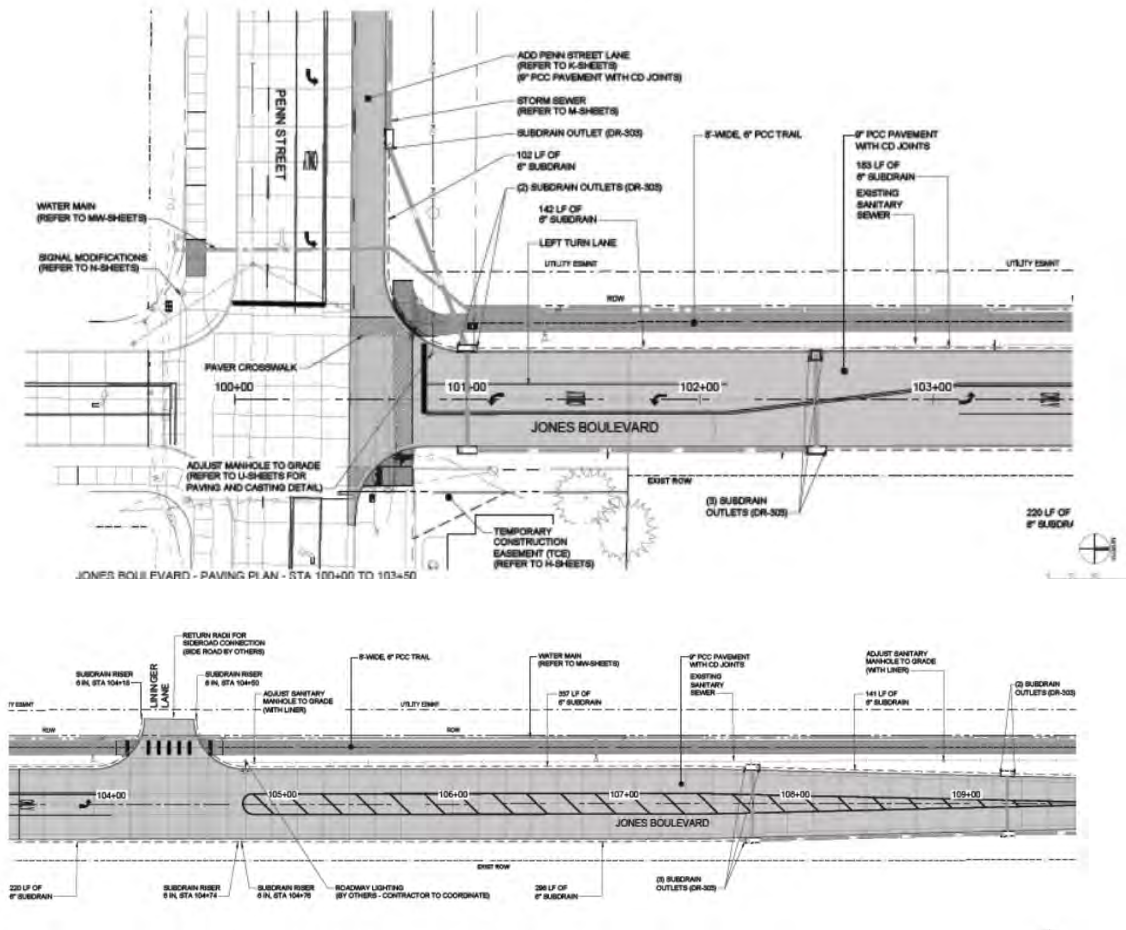
None required.

6. Additional Considerations:

Transportation Network.

Staff has been working with the applicant for approximately two years in regards to developing this property. Now that development of the property is anticipated, the City has authorized the design of North Jones Boulevard. This will be a two-lane urban road with turn lanes onto West Penn Street and the planned street between the commercial and multi-family developments. Westbound West Penn Street will be widened to two full lanes west of the North Jones Boulevard intersection and there will be a left turn onto North Jones Boulevard. Also planned is an 8-foot wide multi-use path along the west

side of the street. Overhead utilities in the area of the proposed road and along 240th Street would be relocated underground. Construction is anticipated to begin in the spring.



Traffic Study.

The City required a traffic study to provide trip generation and distribution assumptions for the proposed development and to determine what intersection geometric and traffic control improvements would be anticipated to be needed for the various phased construction of the proposed development.

The Johnson County Metropolitan Planning Organization staff reviewed the traffic study. It generally agrees with the traffic study, but did provide review comments for amendments.

The traffic study recommends that the intersection of West Penn Street and Country Lane/Saratoga Place. The timing depends on the timing of the second phase of the multi-family development and or the timing and type of commercial development. The recommended location is due to the spacing of the existing signal at West Penn Street and North Jones Boulevard and the lack of turn lane queuing if the proposed signal were to line up with the driveway entrance at LL Pelling Co.

Notably, Saratoga Place is an off-site improvement and will need cooperation from the adjacent property owner to the west. The applicant has been in discussions with the adjacent property owner. The development of the subject property will be subject to the installation of this roadway and traffic signal at some phase of the development. The timing of Saratoga Place and the signal will be articulated in the conditional recommended by City staff and Developer's Agreement for subdivision improvements.

7. Staff Recommendation:

Findings:

1. The preliminary plat, which proposes commercial and residential development, would consistency with the Comprehensive Plan Future Land Map designation, which are Commercial and Residential, respectively; and
2. The preliminary plat, with conditions recommended by City staff, would achieve consistency with Section 180.11(3)(A) and 180.12 of the North Liberty Code of Ordinances, which sets forth the preliminary subdivision plat submittal requirements and design standards, respectively.

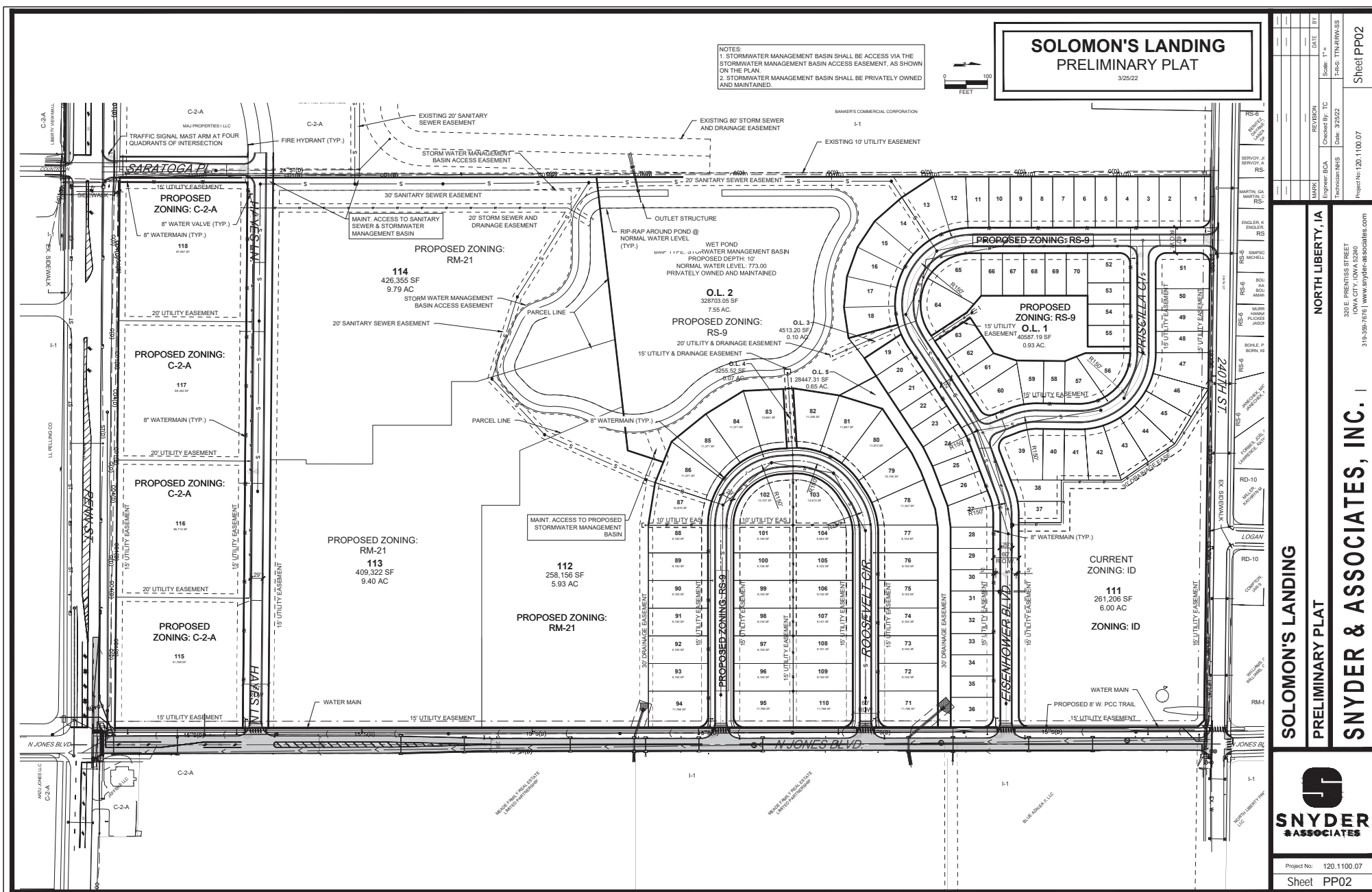
Recommendation:

Staff recommends the Planning Commission accept the two listed findings and forward the request of Solomon Holdings, LLC to approve a Preliminary Subdivision Plat for a 118 lot subdivision on 76.97 acres, more or less, on property located on the north side of West Penn Street west of the North Jones Boulevard as extended northerly from West Penn Street to 240th Street to the City Council with a recommendation for approval subject to the following conditions:

1. That the traffic study be updated to incorporate review comments by Johnson County Metropolitan Planning Organization;
2. That the right-of-way for Saratoga Place be dedicated to the City of North Liberty and roadway and infrastructure improvements constructed thereon by the owner, and accepted by the City prior to the issuance of buildings permits for Lots 116, 117 and 118 if the type of development warrants the additional access in the sole discretion of the City. The City may require an additional traffic study to determine whether the additional access in warranted; and
3. That the right-of-way for Saratoga Place be dedicated to the City of North Liberty, roadway and infrastructure improvements constructed thereon and the traffic signal adjacent to Saratoga Place shall be installed by the owner, and accepted by the City prior to the issuance of buildings permits for Lot 114. All of the aforementioned improvements may be required if the type of development on Lots 116, 117 and 118 warrants the improvements, in the sole discretion of the City. The City reserves the right to require a traffic study to determine whether any or all of the improvements are warranted

Suggested motion:

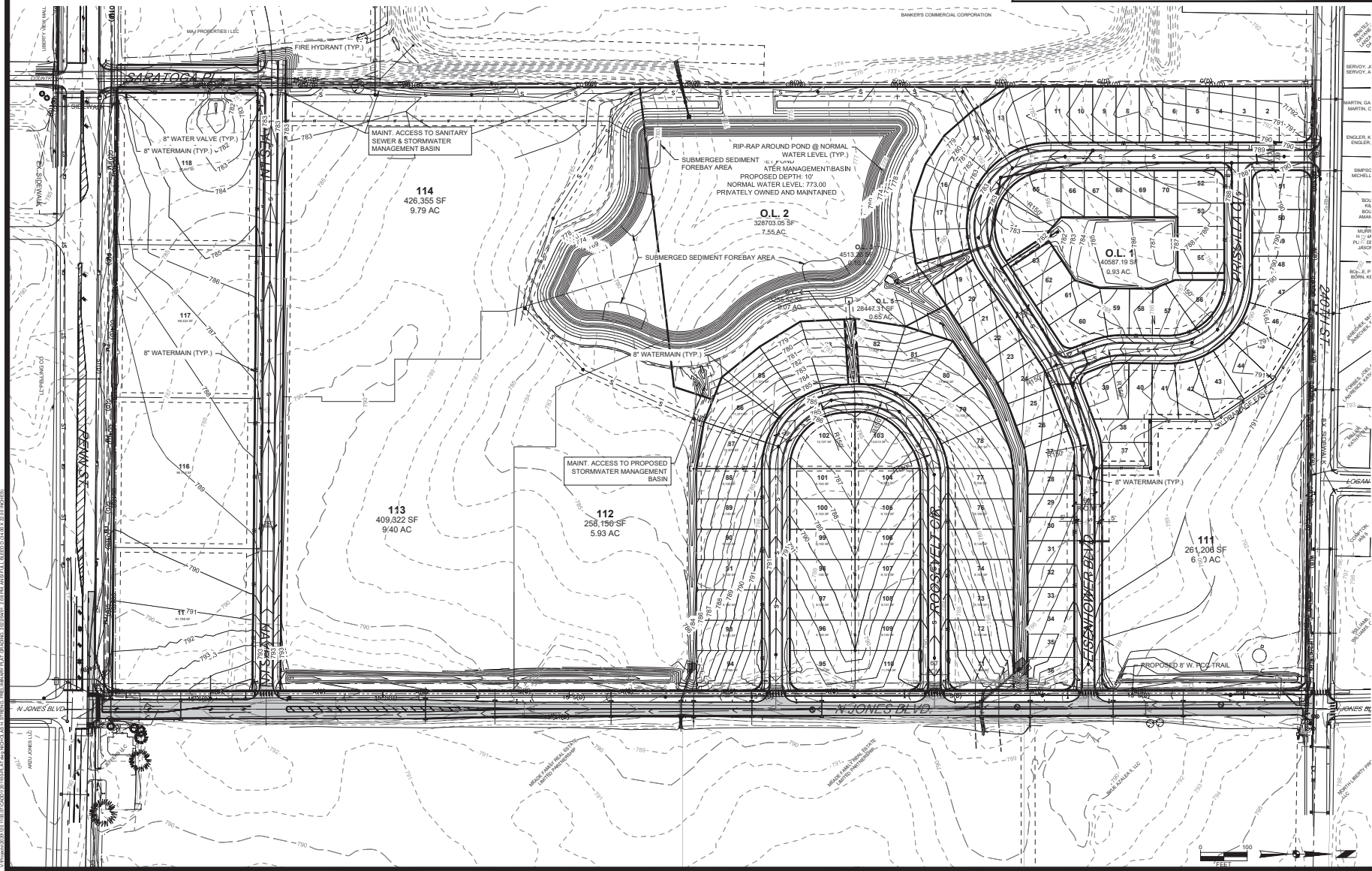
I move that the Planning Commission accept the two listed conditions and forward the Preliminary Plat to the City Council with a recommendation for approval subject to the conditions recommended by City staff.



NOTES:
1. SUBMERGED SEDIMENT FOREBAY AREA SIZING WILL BE INCLUDED IN THE FINAL SWMP AND SWPPP.
2. SUBMERGED SEDIMENT FOREBAY AREAS FINAL LOCATION, SIZING AND DEPTH WILL BE FINALIZED IN FUTURE FINAL PLAT AND CONSTRUCTION PLANS.

SOLOMON'S LANDING
PRELIMINARY PLAT GRADING

3/25/22



MARK		REGION	DATE	BY
Engineer BGA		Checked By: TC	Scale: 1" = 140'	3/25/22
Technician MJS		Date: 3/25/22	1405, TN-RW-3B	Sheet PP03
Project No: 120.1100.07		Project No: 120.1100.07		

SOLOMON'S LANDING
PRELIMINARY PLAT GRADING
SNYDER & ASSOCIATES, INC.

SNYDER & ASSOCIATES
Project No: 120.1100.07
Sheet PP03

SNYDER & ASSOCIATES
320 E. PRENTISS STREET
IOWA CITY, IOWA 52240
319-399-1676 | www.snyder-associates.com

- Biggest Q is when to signalize Penn/Saratoga.
- Table 7 has some discrepancies. Let me know if I missed something.
I'm confused.
↳ thus was not able to follow trip distribution.

- Used 70% factor for TSWA.

**SOLOMON'S LANDING DEVELOPMENT
TRAFFIC IMPACT STUDY**

- If possible, extend LT lane length to 200 ft @ Penn/Saratoga.
North Liberty, Iowa | March 18, 2022
- should not use internal capture - not correct for this type of Dev.



SNYDER
& ASSOCIATES

IOWA | MISSOURI | NEBRASKA | SOUTH DAKOTA | WISCONSIN

SOLOMON'S LANDING DEVELOPMENT TRAFFIC IMPACT STUDY

North Liberty, Iowa | March 18, 2022

Prepared for:

BEN HUR CONSTRUCTION COMPANY
St Louis, Missouri 63125

Snyder & Associates, Inc. Project No. 120.1100.07

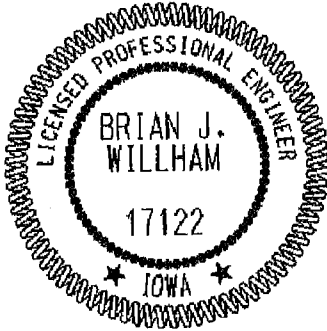
	I hereby certify that this Engineering Document was prepared by me or under my direct personal supervision and that I am a duly Licensed Professional Engineer under the Laws of the State of Iowa.				
	<table><tr><td>Brian J. Willham</td><td>December 31, 2023</td></tr><tr><td colspan="2">License Number 17122</td></tr></table>	Brian J. Willham	December 31, 2023	License Number 17122	
	Brian J. Willham	December 31, 2023			
	License Number 17122				
My License Renewal Date is December 31, 2023.					
Pages or sheets covered by this seal: All					

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Synchro Capacity Analysis and Queue Reports	Appendix H

1. INTRODUCTION

Ben Hur Construction is proposing to develop a currently undeveloped parcel in North Liberty into a mixed-use commercial and residential development. This 80 acre parcel is planned to be a mixed use one, including commercial store properties, mid-rise apartment complexes, detached single-family homes with and potentially an assisted living facility.¹

This study will review anticipated impacts during two phases of project development and projected 2045 conditions and will include:

- Existing and projected traffic volumes at two identified intersections adjacent to the development.
- West Penn Street & North Jones Boulevard.
- West Penn Street & Saratoga/Country Lane.
- A review of intersection crash history.
- Development site and adjacent site trip generation and distribution for current and projected future traffic volumes.
- Signal warrant analysis.
- Turn lane warrant analysis.
- Intersection capacity analysis.

Exhibit 1 shows the study area and **Exhibit 2** shows the Site Plan. Both may be found in the appendix.

2. EXISTING CONDITIONS

The study development is proposed to encompass 80 acres of greenspace in North Liberty, IA, located north of West Penn Street, between the intersections of West Penn Street & North Jones Boulevard to the east and West Penn Street & Country Lane to the west. The parcel is currently used for agricultural production with surrounding land uses including agricultural uses, residential developments, retail locations, manufacturing and industrial facilities, and an educational facility.

West Penn Street is a 70 ft – wide, four lane minor arterial with an urban cross section and a posted speed limit of 35 mph between Country Lane and North Jones Boulevard.

Country Lane is a 425 ft long private access road with an urban cross section that provides access to the Liberty View shopping complex and a private residence. There is no posted speed limit, but median travel speeds are assumed to be 25 mph.

North Jones Boulevard is a 40 ft-wide, two-lane major collector with a two-way center left turn lane, an urban cross section, and a posted speed limit of 35 mph.

The intersection of West Penn Street & Country Lane is a three-approach intersection with a stop control on the minor leg. At the intersection, West Penn Street has a five-lane cross section with a dedicated left turn lane for WB traffic while Country Lane has a two-lane cross section and is without a dedicated turn lane.

¹ The inclusion of a Senior Living Facility to the overall development has been discussed with the City of North Liberty but has not yet been confirmed nor zoned for. This facility type was included in this report as a conservative measure.

The intersection of West Penn Street & North Jones Boulevard is a three-approach intersection with a traffic signal. West of the intersection West Penn Street has a three-lane cross section (two lanes EB and one lane WB). East of the intersection, West Penn Street has a four-lane cross section with a dedicated WB left-turn lane. North Jones Boulevard has a three-lane cross section with a dedicated NB left- and right-turn lane.

a. Traffic Data

Annual Average Daily Traffic (AADT) volumes were recorded by the Iowa DOT in 2014 and 2018 along West Penn Street and on North Jones Boulevard. The AADT values for West Penn Street were recorded near the I-380 & West Penn Street interchange to the west and near the Highway 965/Ranshaw Way & West Penn Street intersection to the east. Projected 2045 traffic volumes were also obtained from the Metropolitan Planning Organization of Johnson County (MPOJC) in March 2022.

Iowa DOT counts were unavailable for Country Lane.

Table 1. Average Daily Traffic, Iowa DOT (veh/day)

	Approach AADT (veh/day)		
	West Penn Street (East of N Jones Blvd)	West Penn Street (West of N Jones Blvd)	N Jones Boulevard (South of W Penn St)
Iowa DOT, 2014	15,100	17,700	4,920
Iowa DOT 2018	15,100	21,990	3,780
MPOJC, 2045 Projected	22,900	22,700	8,800

Intersection turning movements were also recorded on February 16, 2022, by Snyder & Associates.² These lower volumes may be partially explained by reduced traffic volumes on the day the counts were collected, the opening of the I-380 & Forevergreen Road interchange diverting some traffic from the West Penn Street Corridor, or other factors.

Table 2. Average Daily Traffic (veh/day), Snyder & Associates, 2022

	Approach AADT (veh/day)		
	East	West	South
West Penn Street & North Jones Boulevard	10,736	12,276	3,728
West Penn Street & Country Lane	12,193	12,192	257

b. Crash History

In 2021, the Iowa Department of Transportation (Iowa DOT) introduced a new safety performance functions (SPF) methodology to review intersection safety within Iowa. "A safety performance function (SPF) is an equation used to predict the average number of crashes per year at a location as a function of exposure and,

² Counts were factored using Iowa DOT expansion factors to adjust collected volumes with seasonal factors. A factor of 1.029 were applied to collected values.

in some cases, roadway or intersection characteristics. Generally, SPF's more realistically demonstrate the relationship between crashes and traffic volume.”³

The intersection of West Penn Street & North Jones Boulevard is classified by the Iowa DOT as an “Undivided Low Speed Traffic Signal Control” intersection and is noted as a “Tier 3 – Performing Better than Expected” intersection. Compared to similar intersections across the state, the study intersection is within the best performing 75 percent of similar intersections within Iowa (ranked 1,168 out of 1,568 for all crashes). West Jones Street & Country Lane was not classified by the Iowa DOT at the time of this report's drafting.

Beyond the PCR ranking for each intersection, the type and manner of recorded collisions at each intersection was reviewed. Crash reports for the 10-year period between 2011-2021 were reviewed for the study area through the Iowa DOT ICAT crash database.

West Penn Street & Country Lane

There were no recorded collisions at this intersection during the study period.

West Penn Street & North Jones Boulevard

There were 10 recorded collisions with 3 injuries at the West Penn Street & North Jones Boulevard intersection. Most recorded collision types were for rear-ended collisions while the most common crash cause was due to motorists following another too closely. Rear-end collisions comprising the majority of crashes at a signalized intersection is not unexpected as vehicles slow or stop in response to the signal.

Most of the crashes occurred as motorists traveled west on West Penn Street (4 of 10), but 3 crashes were between eastbound traffic, 2 were between northbound motorists, and the remaining crash was between two southbound motorists south of the intersection. None of the recorded crashes were single-vehicle collisions.

The table below summarizes crash data at the study intersections. Additional crash details are in the appendix.

Table 3: Study Intersection Crash History Summary – Year 2011-2021

Intersection	Crashes (Injuries)	Crash Severity	Predominant Crash Types (# Crashes)	Predominant Major Causes (# Crashes)
West Penn Street & North Jones Boulevard	10 (3)	1 – Minor 2 – Possible 7 – PDO*	• Rear-end (8) • Broadside (1) • Sideswipe, Same Direction (1)	• Followed Too Close (5) • Other (2) • Operating Vehicle in a Reckless Manner (1) • Driving Too Fast for Conditions (1) • Made Improper Turn (1)

*Property Damage Only Collision (PDO)

3. PROPOSED DEVELOPMENT

The proposed development is for a mixed-use commercial and residential development on 80 acres in North Liberty. This development will include multiple land uses including commercial store properties, mid-rise apartment complexes, detached single-family homes, and potentially an assisted living facility. The proposed

³ Iowa DOT. Potential for Crash Reduction (PCR) of Intersections Study. Online available at: <https://iowadot.maps.arcgis.com/apps/MapSeries/index.html?appid=6920b9b36fa54caa90c25bd6dcd0c7c>

development is planned to be constructed in three phases, with full site construction to be completed within an eight year period. An illustrative site plan may be found in the appendix.

Due to this quick construction schedule, expected facilities constructed during the first phase (Phase 1) will be considered for Opening Year traffic impacts, Phases 1, 2, & 3 will be considered for cumulative traffic impacts (Phase 3), and a future conditions period including potential adjacent development will be considered for 2045 impacts (2045).

The site is proposed to include the construction of several new roads including adding north approach legs to the existing West Penn Street & Country Lane and West Penn Street & North Jones Boulevard intersections.

4. ANALYSIS OF FUTURE CONDITIONS

a. Background Traffic Growth

Background traffic growth assumptions were obtained from the MPOJC to calculate an annual traffic growth rate of 2 percent. This growth factor was applied to existing traffic volumes observed by Snyder & Associates to generate a background traffic growth estimate. A Phase 3 multiplier of 1.13 and a 2045 multiplier of 1.49 was applied to existing traffic values to generate background traffic growth estimates that were combined with site and offsite traffic generation values to obtain projected traffic volumes in each period. ?

b. Trip Generation

Estimated trip generation for the proposed development was calculated according to the 11th Edition of the ITE *Trip Generation Manual* for weekday AM and PM peak hours for each analysis period. The following ITE Land Use Codes with the relevant independent variable were used for anticipated land uses:

- LU(220) – Single-Family Detached Housing.
 - Variable: Dwelling Units.
- LU(220) – Multi-family Housing (Mid-Rise).
 - Variable: Dwelling Units.
- LU 254 – Assisted Living Facility.
 - Variable: Dwelling Units.
- LU 945 – Convenience Store/Gas Station.
 - Variable: Vehicle Fuel Positions.
- LU(822) Strip Retail (<40k Square Feet). 10th edition does not have a 821 or 822 code
 - Variable: Square footage.
 - (821) – Shopping Plaza 40-150k sq ft passby traffic rates were used for the PM period.
- LU 712 – Small Office Building.
 - Variable: Square Footage.

Due to the development's anticipated construction phasing, Trip Generation Rates were modeled for 3 distinct periods: Phase 1, Phase 3 (which includes all construction planned for the development and the potential assisted living facility), and 2045 (which includes the full site, potential assisted living facility, assumed commercial development to the west of the site, and background traffic growth).

Table 4. Anticipated Site Build Quantities – Phase 1

ITE Code	Land Use Type	Variable
210	Single-Family Detached Housing	20 DU
220 221	Multifamily Housing (Mid-Rise)	156 DU
945	Convenience Store/Gas Station	16 VFP
822	Strip Retail <40K	0 K SQ FT
712	Small Office Building	0 K SQ FT
254	Assisted Living	0 DU

Table 5. Anticipated Site Build Quantities – Phase 3, Cumulative

ITE Code	Land Use Type	Variable
210	Single-Family Detached Housing	110 DU
220 221	Multifamily Housing (Mid-Rise)	428 DU
945	Convenience Store/Gas Station	16 VFP
822	Strip Retail <40K	24 K SQ FT
712	Small Office Building	7 K SQ FT
254	Assisted Living	100 DU

Table 6. Anticipated Offsite Build Quantities, west of Solomon's Landing, 2045

ITE Code	Land Use Type	K SQ FT
821	Shopping Plaza	55

Site traffic consists of three parts: site generated traffic, internally captured traffic, and site passby traffic. Site generated traffic consists of new trips generated by land uses; internally captured traffic occurs when multiple site uses are combined into a single entering/exiting trip, thereby reducing the travel load on the adjacent streets; and passby trips are more trips of opportunity as existing background traffic users divert their travel patterns to take advantage of the new land use.⁴

Am I missing something?

Table 7: Net Site Trip Generation – Phase 1

ITE Code	Quantity	Variable	Land Use	Daily				AM Peak				PM Peak			
				Rate	Total Trips	Entering	Exiting	Rate	Total Trips	Entering	Exiting	Rate	Total Trips	Entering	Exiting
210	20	DU	Single-Family Detached Housing	9.43	189	94	94	0.75	15	4	10	0.99	20	18	6
221	156	DU	Multifamily Housing (Mid-Rise)	4.54	708	354	354	0.35	55	13	38	0.39	61	58	23
254	0	DU	Assisted Living	2.6	-	-	-	0.22	-	-	-	0.33	-	-	-
945	16	VFP	Convenience Store/Gas Station	257.13	4,114	2,057	2,057	68.83	1,101	164	172	53.51	858	262	125
822	0	1k SQ Ft	Strip Retail <40K	54.45	-	-	-	7.6	-	-	-	13.24	-	-	-
712	0	1k SQ Ft	Small Office Building	14.39	-	-	-	2.61	-	-	-	3.15	-	-	-
Total				-	5,011	2,505	2,505	-	401	181	220	-	337	182	155

street
Penn/240th

→ The rate x quantity does not add up for a number of variables ☐

→ In the 10th edition of ITE, the avg. rate for Code 945 VFP is 12-15. The rates shown (68.83/53.5) seem high. These #'s are also different than those in Table 8.

→ What is the internal capture? i.e. of the 1,101 AM trips for gas station, 765 are internal capture trips?

⁴ The displayed trip generation tables were adjusted for internal capture expectations.

Table 8: Net Site Trip Generation – Phase 3

ITE Code	Quantity	Unit	Land Use	Daily				AM Peak				PM Peak					
				Rate	Total Trips	Entering	Exiting	Rate	Total Trips	Entering	Exiting	Rate	Total Trips	Entering	Exiting		
210	110	DU	Single-Family Detached Housing	9.43	1,037	519	519	0.75	83	53	14	39	0.99	109	70	45	25
221	420	DU	Multifamily Housing (Mid-Rise)	4.54	1,943	972	972	0.35	147	38	109	0.39	164	98	66		
251	100	DU	Senior Adult Housing - Single Family	2.6	260	130	130	0.22	22	11	7	4	0.33	33	17	7	9
946	16	VFP	Convenience Store/Gas Station	267.13	4,114	2,057	2,057	27.04	293	147	146	23.88	286	136	113		
822	24	1k SQ Ft	Strip Retail <40K	64.45	1,307	653	653	7.6	131	66	66	13.24	341	172	123		
712	7	1k SQ Ft	Small Office Building	14.39	101	50	50	2.61	18	11	7	3.15	22	9	13		
Total				-	8,762	4,381	4,381	-	654	283	371	-	900	468	348		

Diff rates than previous table.

Rates x quantity don't add up.

Table 9: Net Site Trip Generation – Offsite

ITE Code	Quantity	Variable	Land Use	Daily				AM Peak				PM Peak			
				Rate	Total Trips	Entering	Exiting	Rate	Total Trips	Entering	Exiting	Rate	Total Trips	Entering	Exiting
821	55		Shopping Plaza	94.49	5,197	2,598	2,598	7.06	388	202	186	9.72	535	262	273
Total				-	5,197	2,598	2,598	-	388	202	186	-	535	262	273

c. Trip Distribution

Several distribution assumptions were made to assign site generated trips to the local road network. These assumptions include:

- As West Penn Street provides a direct link to I-380 and is classified as a minor arterial, all commercial site traffic and most of the residential site traffic will use West Penn Street rather than West 240th Street NE.
- Due to intersection proximity, all multi-family dwelling unit trips are assumed to use West Penn Street.
- 50 percent of the trips generated by 20 single family dwelling units constructed during Phases 1 and 2 are assumed to use West Penn Street.
- 50 percent of the trips generated by the 50 single family dwelling units constructed during Phase 3 are assumed to use West Penn Street.
- 90 percent of the 40 single family dwelling units constructed during Phase 3 are assumed to use West Penn Street.
- 50 percent of the senior living center generated trips are assumed to use West Penn Street.
- Residential trips are assumed to have different access point splits, with multi-family dwelling unit trips to be split evenly across both intersections while single family dwelling unit trips and senior living facility trips are assumed to access the site via West Penn Street & North Jones Boulevard.
- Commercial trips are assumed to split evenly across both study intersections.
- Site travel will mimic existing observed travel flow for AM and PM peak periods. As the two study intersections are without a north approach leg, existing south approach traffic travel flows percentages will be used to approximate site exiting traffic flows.
- A small amount of additional traffic was added to the intersection of West Penn Street & North Jones Boulevard to reflect anticipated use of the collector street by individuals traveling southbound. This additional volume was applied equally across the SBL/T/R, NBT, EBL, and WBR turning movements.

How much?



Table 10. Observed Traffic Directional Flow

Traffic Directional Flow	AM Peak Period		PM Peak Period	
	West Penn Street & Country Lane	West Penn Street & North Jones Boulevard	West Penn Street & Country Lane	West Penn Street & North Jones Boulevard
EB	38%	37%	55%	52%
WB	61%	45%	44%	33%
NB	0%	18%	0%	15%

Turning moment percentages and volumes for all studied periods are available in the appendix as is a site diagram with assumed dwelling unit and commercial property directional flows.

d. Signal Warrant Analysis

Traffic signal warrants for the proposed intersection of West Penn Street & Saratoga/Country Lane were reviewed for existing and projected future traffic volumes. The planning-level signal warrant analysis uses a typical hourly traffic distribution to evaluate Warrant 1 (8-hour volume warrant) and peak hour traffic volumes to evaluate Warrant 3 (peak hour warrant) from the *Manual on Uniform Traffic Control Devices* (MUTCD).

Warrant 1 was evaluated because it is typically the strongest indicator of a potential signal need. Warrant 3 was evaluated where traffic conditions may create undue delay on minor streets due to major street traffic.

Upon review, Warrant 1 (condition B) and Warrant 3 were both met under projected volume conditions during Phases 1 & 3 and 2045 traffic volume conditions.

Refer to the appendix for the planning-level signal warrant calculations.

e. Capacity Analysis

Intersection capacity analysis was completed using the Highway Capacity Manual 6th Edition (HCM 6) and signalized intersection capacity analysis methodology incorporated into the traffic analysis software Synchro 11. Intersection delay is a function of traffic factors such as traffic volume, turning vehicles, vehicle types and arrival patterns, as well as geometric factors such as number of lanes, and traffic control. Intersection operations are categorized by equating average vehicular delay to level of service (LOS) criteria.

LOS A is considered the most desirable level, with the least delay, while LOS F experiences the most delay. Typically, LOS D is the minimum acceptable LOS for urban intersections with LOS E occurring at non-signalized side-street approaches to arterials. LOS criteria defined by the HCM for unsignalized and signalized intersections are shown in the Table below.

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Table 11: Level Service (LOS) Definition

LOS	Average Delay per Vehicle (sec)	
	Unsignalized Intersection	Signalized Intersection
A	Less than 10	Less than 10
B	10-15	10-20
C	15-25	20-35
D	25-35	35-55
E	35-50	55-80
F	Greater than 50	Greater than 80

Capacity Analysis for each study intersection were initially reviewed with both study intersections. For each intersection, a north approach with a dedicated left-turn lane with 150 of storage space and a through/right turn lane was included. An EB left turn lane with 200 ft of storage space was also added to the West Penn Street & Saratoga/Country Lane intersection. *at the Jones intersection?*

Existing Traffic Control Conditions

West Penn Street & Saratoga/Country Lane

Phase 1

In projected Phase 1 conditions, SBL turning movements from West Penn Street & Saratoga/Country Lane exhibited an unacceptable LOS of “F” during the PM peak period, with SBL.

Phase 3

In projected Phase 3 conditions, SBL turning movements from West Penn Street & Saratoga/Country Lane exhibited an unacceptable LOS of “F” during the AM and PM peak periods, with SBL queue lengths exceeding the provided storage space in the PM peak period.

2045

In projected 2045 conditions, SBL turning movements from West Penn Street & Saratoga/Country Lane exhibited an unacceptable LOS of “F” during the AM and PM peak periods, with SBL queue lengths exceeding the provided storage space in the PM peak period. The NBL movement also exhibited an unacceptable LOS of “F” during the PM period.

West Penn Street & North Jones Boulevard

Phase 1

In projected Phase 1 conditions, existing intersection control (signalized) and proposed turn lane configuration conditions were adequate to manage projected traffic volumes and queue lengths.

Phase 3

In projected Phase 3 conditions, existing intersection control (signalized) and proposed turn lane configuration conditions were adequate to manage projected traffic volumes and queue lengths. Traffic signal timing will need to be adjusted to accommodate the new traffic patterns and volumes.

2045

In projected 2045 conditions, the NBL movement exhibited an unacceptable LOS of “F” in the PM peak period. The WBT through movement also exhibited an unacceptable LOS of “F” in the AM peak period.

Alternative Conditions Reviewed

To address the projected unacceptable LOS and queue lengths projected under the original review criteria, alternative traffic controls and lane configurations were reviewed.

West Penn Street & Saratoga/Country LanePhase 1

Adding a traffic signal resulted in an acceptable LOS level and projected queue lengths for all intersection movements.

Phase 3

Adding a traffic signal resulted in an acceptable LOS level and projected queue lengths for all intersection movements.

2045

Adding a traffic signal resulted in acceptable LOS levels and projected queue lengths for all intersection movements.

West Penn Street & North Jones Boulevard2045

Adding an additional WB through lane to the West Penn Street & North Jones Boulevard intersection for the 2045 scenario restored acceptable LOS levels for all modeled movements.

*What about NBL movement?
Is this physically possible w/
the location of the building at 1180?*

f. Access Spacing

As described by the Federal Highway Administration, access management refers to the design, application and control of entry and exist points along a road.” This includes intersections with other roads and driveways that serve adjacent properties. Thoughtful access management along a corridor can simultaneously enhance safety for all modes, facilitate walking and biking, and reduce trip delay and congestions.

The upstream functional distance (UFD) of full access intersections is based on motorist perception/reaction time (based on urban/rural character), deceleration distance (based on travel speed), and queue storage length for waiting vehicles (differs based on traffic control method).

Along West Penn Street, the UFD for unsignalized intersections is 415 ft at minimum and for signalized intersections is 515 ft at minimum. Conservatively, 600 ft should be assumed to be an acceptable distance.

The two existing intersections are approximately 1,350 feet apart from each other and are in conformance with SUDAS spacing recommendations.

5. CONCLUSIONS & RECOMMENDATIONS

The proposed Solomon's Landing Development will entail the construction of new north approaches for existing intersections along West Penn Street at Country Lane and North Jones Boulevard.

The West Penn Street & Saratoga/Country Lane intersection is anticipated to have unacceptable performance during reviewed peak hour periods in all Phases of site development. This need is primarily due to the inclusion of a convenience store/gas station in Phase 1 of construction, the expected traffic generation of the site, and this report's distribution assumptions.

If the convenience store/gas station is in the commercial parcels close to Saratoga, it is anticipated that a signal will be needed prior to the completion of Phase 1 of development, as was shown in our modeling. If the convenience store/gas station is located adjacent to North Jones Boulevard, motorists will likely prioritize the existing West Penn Street & North Jones Boulevard intersection, likely delaying the need for a traffic signal until Phase 3 of development.

The use of the existing West Penn Street & Country Lane as the new full access intersection for the west side of the Solomon's Landing Development conforms with established SUDAS spacing guidance. The intersection location also provides safe and efficient access to the future development to the west of Solomon's Landing as well as the existing retail south of West Penn Street.

The West Penn Street & North Jones Boulevard intersection is anticipated to have acceptable performance through Phase 3 of the Solomon's Landing Development but is anticipated to have unacceptable performance for the NBL movement and the WBT movement in 2045. The addition of a second WBT lane on West Penn Street is recommended to be pursued to alleviate these unacceptable conditions.

what about NSC movement?

As reported in their 2017 Long Range Transportation Plan "Future Forward", MPOJC anticipates that West Penn Street will experience significant congestion during peak hours (LOS of E or F) in 2045 scenarios⁵. This report's traffic modeling conforms with the MPOJC's traffic projections.

The following intersection configurations are recommended for both study intersections:

West Penn Street & Saratoga/Country Lane

Prior to the completion of Phase 1 construction

- The installation of a traffic signal (if the convenience store/gas station is proximate to this intersection).
- The construction of a North approach with a dedicated left-turn lane with 150 ft of storage space and a through/right turn lane.

Queues just exceed 200 ft. Why not install 200 ft turn lane?

Prior to the completion of Phase 3 construction

- The installation of a traffic signal, if not installed during Phase 1 of construction.

⁵ Metropolitan Planning Organization of Johnson County. Future Forward: 2045 Long Range Transportation Plan, p. 76. May, 2017. Online Available at: <https://www8.iowa-city.org/WebLink/ElectronicFile.aspx?dbid=0&docid=1786006&>

West Penn Street & North Jones Boulevard

Prior to the completion of Phase I construction

- The construction of a North approach with a dedicated left-turn lane with 150 ft of storage space and a through/right turn lane.
- The construction of a EB left-turn lane with 200 ft of storage space.

Prior to the 2045 Design Year

- The addition of a second WB through lane.

Appendix

Study Area

Site Plan

Site Distribution

Capacity Analysis Summary Tables

Trip Distribution Diagrams

Crash History Reports, Iowa DOT

Planning-Level Signal Warrant Analysis

Synchro Capacity Analysis and Queue Reports



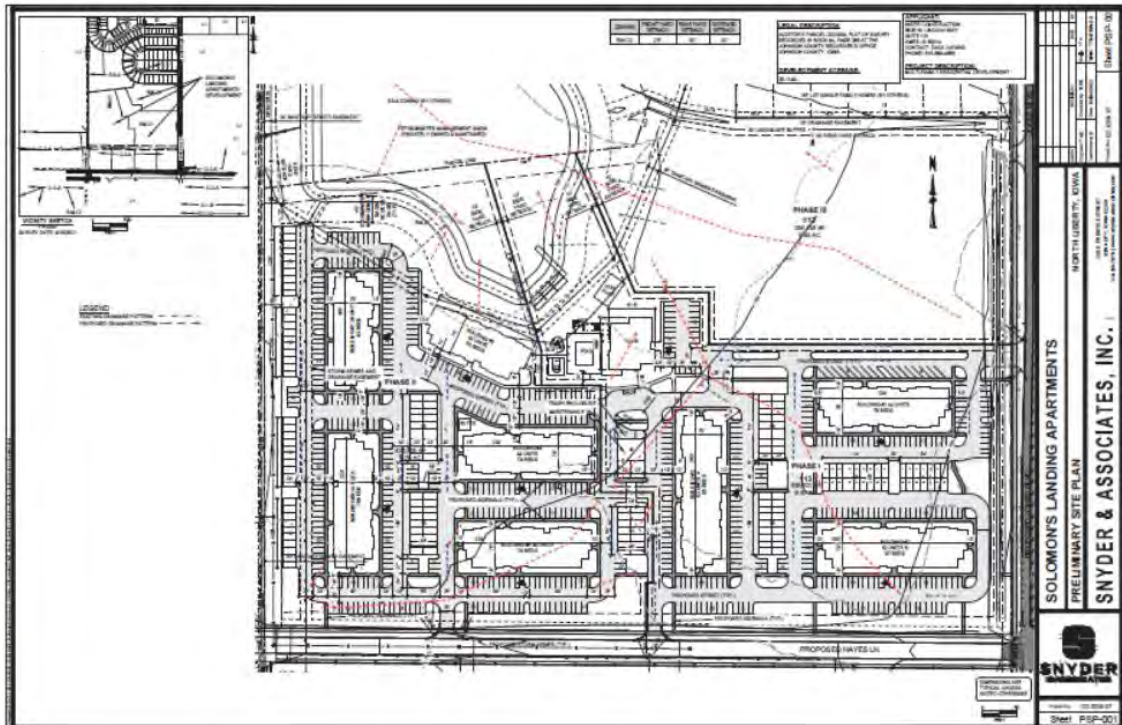
To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of Insite Construction to approve a Site Plan for 352 multi-unit residences in eight buildings and related recreational amenities and site improvements on approximately 25.11 acres of property located on the west side of North Jones Boulevard as extended northerly from West Penn Street to 240th Street.**

North Liberty City staff has reviewed the subject submission, and offer comments presented in this memo. The staff review team includes the following personnel:

Ryan Heiar, City Administrator
Tracey Mulcahey, Assistant City Administrator
Grant Lientz, City Attorney
Tom Palmer, City Building Official
Kevin Trom, City Engineer
Ryan Rusnak, Planning Director

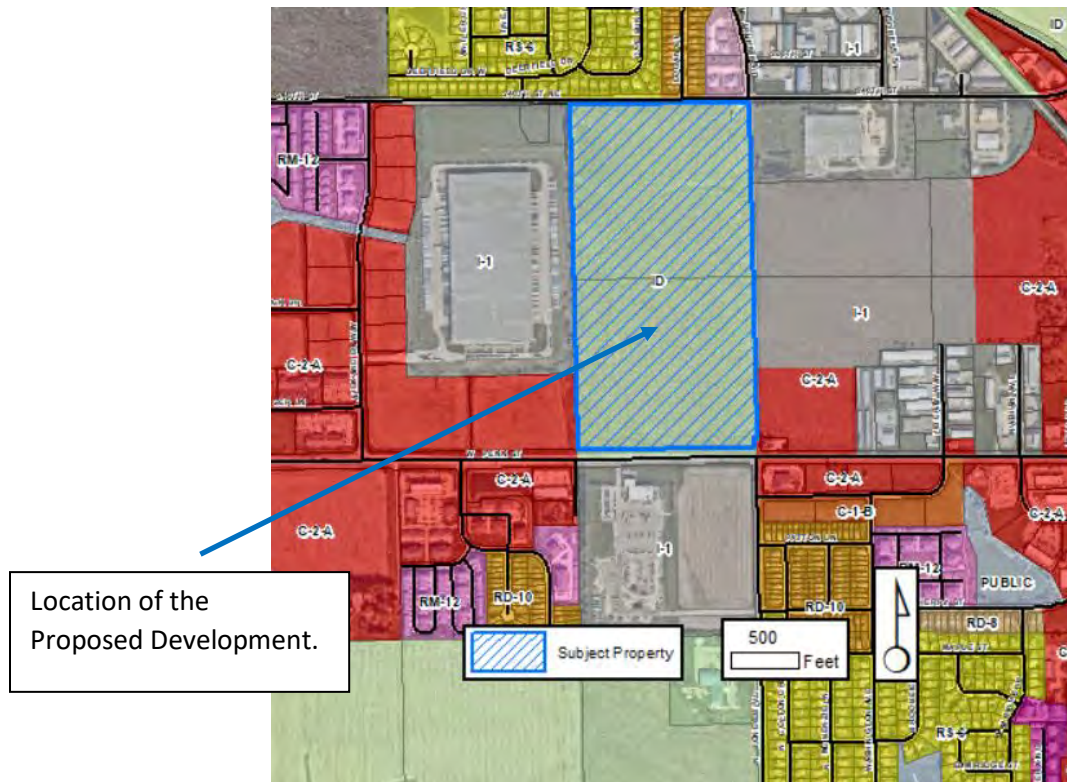
1. Request Summary:

The site plan proposes to construct a 352 multi-unit residences in eight buildings and related recreational amenities and site improvements on approximately 25.11 acres of property.



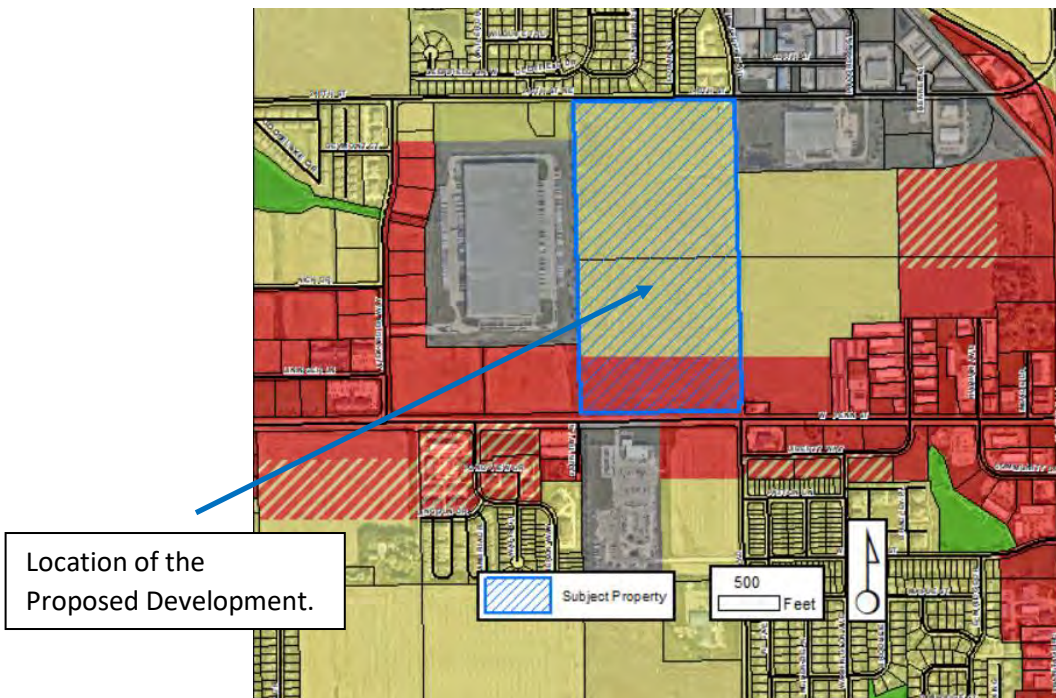
2. Existing Zoning:

The property is currently zoned ID Interim Development District. The property owner is requesting a rezoning to RM-21 Multiple-Unit Residence District on 26.85 acres. It is anticipated that the preliminary site plan would be considered at the same meeting as City Council's final approval of the rezoning.



Proposed zoning districts.

3. Comprehensive Plan Future Land Use Map Designation: Residential.



The proposed zoning and site plan would be consistent with the Future Land Use Map

4. Public Input:

No public comments have been received.

5. Approval Standards:

Section 165.05(2) of the North Liberty Code of Ordinances entitled, "Preliminary Site Review" sets forth the approval standards (Ordinance language in *italics* and staff analysis in **bold**).

- *Date, north arrow and graphic scale.* **This information has been provided on the preliminary site plan.**
- *The property owner's name and description of proposed development.* **This information has been provided on the preliminary site plan.**
- *A vicinity sketch showing the location of the property and other properties within 1,000 feet of it.* **This information has been provided on the preliminary site plan.**
- *Property boundary lines, dimensions, and total area.* **This information has been provided on the preliminary site plan.**
- *Contour lines at intervals of not more than five feet, City datum. If substantial topographic change is proposed, the existing topography shall be illustrated on a separate map and the proposed finished topography shown on the site plan.* **This information has been provided on the preliminary site plan.**
- *The location of existing streets, sidewalks, easements, utilities, drainage courses.* **This information has been provided on the preliminary site plan.**
- *The total square feet of building floor area, both individually and collectively.* **This information has been provided on the preliminary site plan.**

- *All structures and major features shall be fully dimensioned including distance between structures, distance between driveways, parking areas, property lines and building height. **This information has been provided on the preliminary site plan and the architectural renderings.***
- *Off-street parking areas, ingress and egress to the property, number of parking spaces proposed, number of parking spaces required by this code and type of surfacing. **This information has been provided on the preliminary site plan.***
- *Pedestrian walkways with special consideration given to pedestrian safety. **This information has been provided on the preliminary site plan. There are several dedicated pedestrian walkways along internal pedestrian circulation within the development and the public sidewalks.***
- *Trash and refuse enclosures. **This information has been provided on the preliminary site plan.***
- *The general drainage pattern and location of storm water detention features. **This information has been provided on the preliminary site plan.***
- *The general location, type and size of landscaping and ground cover illustrated in color perspective. **This information has been provided on the preliminary site plan and the landscape rendering.***
- *A rendering, elevation or photo of the proposed development. **This information has been provided on the architectural rendering.***

There proposed site plan depicts the northernmost driveway entrance in conflict with the proposed North Jones Boulevard storm intake. The City Engineer has indicated to the applicant on several occasions that this is not acceptable. This will need to be addressed during design on the construction site plan approval.

North Liberty Code of Ordinance Section 168.07(26)(B) sets forth the design standards for multi-unit residences.

B. Use Standards.

- (1) Buildings must be designed with consistent materials and treatments that wrap around all building elevations. There must be a unifying architectural theme for the entire multi-unit development, utilizing a common vocabulary of architectural forms, elements, materials, or colors in the entire structure.
- (2) Building facades must include windows, projected or recessed entrances, overhangs, and other architectural features. Three-dimensional elements, such as balconies and bay windows, are encouraged to provide dimensional elements on a facade.
- (3) Minimum required masonry on front and corner side yard building elevations is 25%,
- (4) A 15% minimum transparency requirement applies to any facade facing a street and is calculated on the basis of the entire area of the facade.

It is staff's opinion that the proposed buildings would achieve consistency with the aforementioned use standards.

North Liberty Code of Ordinances Section 169.10 entitled, "Design Standards" sets forth certain design standards (applicable Ordinance language in italics and staff analysis in bold).

Requirements for All Districts.

- Building design shall be visually harmonious and compatible with the neighborhood character.
- Buildings located on property with double frontages shall have similar wall design facing both streets.
- Except for RS RD, R-MH and ID districts, color schemes shall be primarily based on earth tones. Earth tone colors include colors from the palette of browns, tans, greys, greens, and red. Earth tone colors shall be flat or muted. Building trim and accent areas may feature non-earth tone and brighter colors. In any district, the use of high intensity colors, neon or fluorescent color and neon tubing is prohibited.
- Except in the R-MH district, a minimum roof pitch of 5:12 shall apply to gable, hip, or shed roofs and there shall be a minimum roof overhang at the eaves of 12 inches. This does not apply to portions of a roof that are separate from the structure's primary roof. Metal roofs shall not be corrugated or similar appearance. The color of the roof shall be visually harmonious and compatible with the building color scheme.

It is staff's opinion that the proposed buildings would achieve consistency with the aforementioned use standards.

6. Staff Recommendation:

Findings:

1. The multi-unit residence use of the property would be consistent with the current RM-21 District and the Comprehensive Plan Future Land Use Map designation of Residential; and
2. The site plan would achieve consistency with North Liberty Code of Ordinances Section 165.05(2) entitled, "Preliminary Site Plan Review", Section 168.07(26)(B) and Section 169.10 entitled "Design Standards" and other Code of Ordinance requirements.

Recommendation:

Staff recommends the Planning Commission accept the two listed findings and forward the request to approve a site plan for 352 multi-unit residences in eight buildings and related recreational amenities and site improvements on approximately 25.11 acres of property located on the west side of North Jones Boulevard as extended northerly from West Penn Street to 240th Street to the City Council with a recommendation for approval subject to the condition recommended by City staff.

1. The conflict with the northernmost driveway entrance in conflict and the North Jones Boulevard storm intake be addressed during design on the construction site plan approval.

Suggested motion:

I move that the Planning Commission accept the two listed findings and forward the site plan to the City Council with a recommendation for approval with one condition recommended by City staff.



EXISTING DRAINAGE PATTERN → → →
PROPOSED DRAINAGE PATTERN → → →



ZONING	FRONT YARD SETBACK	REAR YARD SETBACK	SIDEYARD SETBACK
RM-21	25'	30'	10'

LEGAL DESCRIPTION
AUDITOR'S PARCEL 2021009, PLAT OF SURVEY
RECORDED IN BOOK 64, PAGE 285 AT THE
JOHNSON COUNTY RECORDER'S OFFICE,
JOHNSON COUNTY, IOWA

DEVELOPMENT ACREAGE:
25.11AC.

APPLICANT:
INSITE CONSTRUCTION
5820 W. LINCOLN WAY
SUITE 101
AMES, IA 50014
CONTACT: ZACK LUDWIG
PHONE: 515-580-4858

PROJECT DESCRIPTION:
MULTI-FAMILY RESIDENTIAL DEVELOPMENT

MARK		REVISION	DATE	BY
		Engineer: TAC	Checked By: NH-S	
		Technician: CP	Date: 03/04/2022	
			Scalar: T =	
			L-A-S: TTN-RW-S-S	

Project No: 122.0236.07

Sheet PSP-00

NORTH LIBERTY, IOWA

320 E. PRENTISS STREET
IOWA CITY, IOWA 52240
319-359-7676 | www.snyder-asociates.com

SOLOMON'S LANDING APARTMENTS
PRELIMINARY SITE PLAN
SNYDER & ASSOCIATES, INC.



Project No:	122.0239.07
Sheet	PSP-001





To **City of North Liberty Planning Commission**
From **Ryan Rusnak, AICP**
Date **April 1, 2022**
Re **Request of the City of North Liberty for an Ordinance for an Ordinance amending Section 167.01 regarding the definition of "Masonry, Required" and Section 168.07 regarding use standards for garages attached to dwellings and scrivener errors in use standards for vehicle dealerships.**

North Liberty City staff has reviewed the subject submission, and offer comments presented in this memo. The staff review team includes the following personnel:

Ryan Heiar, City Administrator
Tracey Mulcahey, Assistant City Administrator
Grant Lientz, City Attorney
Tom Palmer, City Building Official
Kevin Trom, City Engineer
Ryan Rusnak, Planning Director

Discussion:

This Ordinance cleans up oversights in the large Zoning Ordinance amendment, which was recently amended. As mentioned during the initial amendment, staff is committed to making amendments to achieve the desired community standard.

1. Section 167.01 General Definitions.

It was realized that an outright prohibition on overhead doors was onerous when transparency in the overhead door could provide visual interest to a building. In that light, it is staff's opinion that credit should not be given to solid doors.

"Masonry, required" means the total area of a building elevation measured between grade and the highest point of the coping of a flat roof or the eaves for gable, hip, gambrel or mansard roof. The transparency from windows ~~Windows~~ and doors may be subtracted out of the total area ~~with the exception of overhead doors~~ in non-residential (including mixed use) buildings.

2. Section 168.07 Uses Defined And Use Standards.

A. Section 168.07(27)(B)(9) - Dwelling - Single-Unit.

It was realized that 50% was too restrictive and actually made homes non-conforming, which was not the goal of the amendment. 60% is much more reasonable standard.

- (9) Front-facing garages shall not exceed 16' or ~~50~~60% the width of the front building line, whichever is greater. Garage width is measured between the edges of the garage door; in the case of garages designed with multiple garage doors, the distance is measured between the edges of the outermost doors.

B. Section 168.07(76)(B)(1) - Vehicle Dealership, New and Used

This corrects and outdated code reference.

- (1) The number and location of vehicles shall be designated on an approved site plan. The vehicle display area shall be designed in accordance with the provisions of Section 169.01 and ~~169.13~~169.02 with the exception that display areas do not need to be delineated with paint or other permanent materials.

C. Section 168.07(77)(B)(1) - Vehicle Dealership, Used

This corrects and outdated code reference.

- (1) The number and location of vehicles shall be designated on an approved site plan. The vehicle display area shall be designed in accordance with the provisions of Section 169.01 and ~~169.13~~169.02 with the exception that display areas do not need to be delineated with paint or other permanent materials.

Public Input:

No public input has been received. However, these amendments are the result of applications of the Zoning Ordinance on real projects.

Staff Recommendation:

Finding:

1. The proposed Ordinance would revise oversights in the recently adopted Zoning Ordinance amendment.

Recommendation:

Staff recommends the Planning Commission accept the listed finding and forward the Ordinance amendment to the City Council with a recommendation for approval.

Suggested Motion:

I move that the Planning Commission accept the listed finding and forward the Ordinance amendment to the City Council with a recommendation for approval.



Planning Commission
March 1, 2022

Call to Order

This meeting is being held online via Zoom due to the Council Chambers being unavailable. The space is being used as a polling place for a special election that does not conclude until 8:00 p.m. Other potential city facilities were not available as well. The City has no other facilities that would be practical and reasonable to use for a public meeting. Public input was sought through various means prior to the meeting.

Planning Commission Chair Becky Keogh called the March 1, 2022 Planning Commission to order at 6:30 p.m. Commission members present: Barry A'Hearn, Jason Heisler, Becky Keogh, Patrick Staber, Brian Vincent, Dave Willer; absent: Barry A'Hearn, Josey Bathke.

Others present: Ryan Rusnak, Tracey Mulcahey, Grant Lientz, Kevin Trom, Brandon Pratt, Jon Marner from MMS Consultants, Matt Lepic and other interested parties.

A'Hearn arrived at 6:31 p.m.

Approval of the Agenda

Staber moved, Willer seconded to approve the agenda. The vote was all ayes. Agenda approved.

Solomon Holdings, LLC Zoning Map Amendment

Staff Presentation

Rusnak presented the request of Solomon Holdings, LLC for a zoning map amendment (rezoning) on approximately 71.48 acres from ID Interim Development District to RS-9 Single-Unit Residence District, RM-21 Multiple-Unit Residence District and C-2-A Highway Commercial District. The property is located on the north side of West Penn Street west of North Jones Boulevard as extended northerly from West Penn Street to 240th Street. Staff recommends approval of the application with the following two findings:

1. The proposed zonings would be consistent with the North Liberty Comprehensive Plan Land Use Plan; and
2. The proposed use and density of the development would be compatible with the area;

and with the following conditions:

1. That the preliminary plat be approved at the time of the rezoning.
2. The preliminary site plan for the RM-21 portion of the development be approved at the time of the rezoning.

Applicant Presentation

Brandon Pratt, the applicant, was present and provided additional information. Pratt is looking to provide new housing types at an affordable price point. The proposed RM-21 will have more amenities than other complexes in the area.

Public Comments

Fifteen people attended the Good Neighbor meeting with questions, not concerns. The only concern received was protection of the trail for users.

Questions and Comments

The Commission discussed the application including the extension of Jones Boulevard, the location of trail, RM-21 access points, the bridal shop location in relation to the new roadway, potential need for signalization on Penn Street at Saratoga, use of RS-9 zoning in this development, accesses to multi family, size of home on RS-9, the potential price point of homes, and the location of a park in this area.

Recommendation to the City Council

Willer moved, A'Hearn seconded that the Planning Commission accept the two staff recommended findings: the proposed zonings would be consistent with the North Liberty Comprehensive Plan Land Use Plan; and the proposed use and density of the development would be compatible with the area; and forward the zoning map amendment with the following conditions:

1. That the preliminary plat be approved at the time of the rezoning.
2. The preliminary site plan for the RM-21 portion of the development be approved at the time of the rezoning.

to the City Council with a recommendation for approval. The vote was: ayes – Keogh, Heisler, Vincent, A'Hearn, Willer, Staber; nays – none; absent – Bathke. Motion approved.

Matnic, LLC Zoning Map Amendment

Staff Presentation

Rusnak presented the request of Matnic, LLC for a zoning map amendment (rezoning) on approximately 1.48 acres from RS-3 Single-Unit Residence District to RM-21 Multiple-Unit Residence District on property located at 10 Jaro Way. Staff recommends approval with two findings:

1. The proposed zonings would be consistent with the North Liberty Comprehensive Plan Land Use Plan; and
2. The proposed use and density of the development would be compatible with the area.

Applicant Presentation

Jon Marner, MMS Consultants and Matt Lepic, the developer, were present and offered additional information on the application.

Public Comments

Two groups attended the Good Neighbor Meeting. Three emails were received in objection to the project regarding the development being rentals and requests to maintain the residential structure on the property.

Questions and Comments

The Commission discussed the application including moving the home on the property, traffic concerns in the area, potentially updating the crosswalk on Front Street with a flashing sign and the connection with neighboring development.

Recommendation to the City Council

Heisler moved, Willer seconded that the Planning Commission accept the two staff recommended findings, the proposed zonings would be consistent with the North Liberty Comprehensive Plan Land Use Plan; and the proposed use and density of the development would be compatible with the area; and forward the zoning map amendment to the City Council with a recommendation for approval. The vote was: ayes – Staber, Heisler, A’Hearn, Keogh, Willer, Vincent: nays – none; absent – Bathke.

Approval of Previous Minutes

A’Hearn moved, Staber seconded to approve the minutes of the February 1, 2022 meeting. The vote was all ayes. Minutes approved.

Old Business

No old business was presented.

New Business

Rusnak updated the Commission on the status of the Comprehensive Plan. Keogh will not be attending the next two meetings due to a conflict.

Adjournment

At 7:22 p.m., A’Hearn moved, Willer seconded to adjourn. The vote was all ayes. Meeting adjourned.

Signed:

Tracey Mulcahey, City Clerk